

Steve Cammarata, Chair
Michael Graf, Vice-Chair
Monica Dever, Commissioner
Jay Mattingly, Commissioner
Joaquin Santos, Commissioner
Bob Steinbach, Commissioner
Brenda Stephens, Commissioner



Lomita City Hall
Council Chambers
24300 Narbonne Avenue
Lomita, CA 90717
Phone: (310) 325-7110
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Next Resolution No. PC 2023-14

**AGENDA
REGULAR MEETING
LOMITA PLANNING COMMISSION
MONDAY, JUNE 12, 2023
6:00 P.M.**

THE PLANNING COMMISSION HAS RESUMED PUBLIC MEETINGS IN THE COUNCIL CHAMBERS. PARTICIPATION BY MEMBERS OF THE PUBLIC IS ONLY GUARANTEED VIA IN-PERSON ATTENDANCE.

AS A COURTESY, THE CITY WILL ATTEMPT TO ALSO ALLOW PUBLIC PARTICIPATION DURING THE MEETING VIA A COMPUTER OR SMART DEVICE USING THE FOLLOWING ZOOM LINK:

<https://us06web.zoom.us/j/89702923785>

Telephone Option: (669) 900-6833 Meeting ID: 897 0292 3785

Please note that the City cannot, and does not, guarantee that the above Zoom link or dial-in feature will work, that any individual commenter's computer or smart device will operate without issue, or that the City's hosting of the Zoom meeting will work without issue. Members of the public acknowledge this and are on notice that public participation is only guaranteed via attendance in Council Chambers and that the Zoom option is provided as a courtesy only. Technological issues or failure of the Zoom link to be operational for any reason will not result in any pause, recess, or cancellation of the meeting.

If you wish to provide public comment during Oral Communications or for a particular agenda item, you may either contact the Deputy City Clerk before the meeting, at 310-325-7110 ext. 141, complete a speaker card and give it to the Deputy City Clerk before or during the meeting, or if participating via Zoom, utilize the "raise hand" function to join the queue to speak when the Chair calls the item for discussion. Your name and city of residency is requested, but not required.

1. OPENING CEREMONIES

- a. Call Meeting to Order
- b. Flag Salute
- c. Roll Call

2. ORAL COMMUNICATIONS

Persons wishing to address the Planning Commission on subjects other than those scheduled are requested to do so at this time. Please provide your name and address for the record. In order to conduct a timely meeting, a 3-minute time limit per person has been established. Government Code Section 54954.2 prohibits the Planning Commission from discussing or taking action on a specific item unless it appears on a posted agenda.

3. CONSENT AGENDA

- a. **APPROVAL OF MINUTES:** May 8, 2023

RECOMMENDED ACTION: Approve minutes.

4. PUBLIC HEARINGS

- a. **AMENDMENT TO SITE PLAN REVIEW NO. 1049; ZONE VARIANCE NO. 246; MINOR CONDITIONAL USE PERMIT NO. 5 FOR 2 UNPERMITTED UNITS TOTALING 1,658 FT², THE UNPERMITTED EXPANSION OF UNITS 204, 205, AND 206, A 256-SQUARE-FOOT PATIO, TO ALLOW UP TO 50 PERCENT OF THE GROUND FLOOR TO BE OCCUPIED BY A COMBINATION OF USES IDENTIFIED IN LOMITA MUNICIPAL CODE SEC. 11-1.48.02(B); VARIANCE TO REMOVE THE LOADING ZONE REQUIREMENT IN ORDER TO ADD REGULAR PARKING SPACES; AND FOR A SHARED PARKING ARRANGEMENT AT 25820 LUCILLE AVENUE WITHIN THE COMMERCIAL RETAIL (C-R) ZONE; AND FINDING THE ACTION TO BE EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT**

APPLICANT: Jeff S. Coffman, on behalf of property owner David Nosrati / Maxnet Commercial, LLC

PRESENTED BY: Assistant Planner Barbero

RECOMMENDED ACTION: Adopt a resolution amending Site Plan Review No. 1049 to legalize unpermitted construction and to allow a combination of uses identified in LMC Sec. 11-1.48.02(B) up to 50 percent, approve a variance to allow the removal of the loading zone for additional parking, approve a minor conditional use permit for shared parking at 25820 Lucille Avenue in the Commercial Retail (C-R) Zone, and confirm the categorical exemption from the California Environmental Quality Act (CEQA).

5. SCHEDULED MATTERS

- a. **COMMUNICATIONS REGARDING CITY COUNCIL ACTIONS**

6. OTHER MATTERS

- a. **STAFF ANNOUNCEMENTS**
- b. **PLANNING COMMISSIONER ITEMS**

7. COMMISSIONERS TO ATTEND CITY COUNCIL MEETINGS

Tuesday, July 4, 2023, City Council meeting is cancelled

Tuesday, July 18, 2023, City Council meeting

8. ADJOURNMENT

The next regular meeting of the Planning Commission is scheduled for Monday, July 10, 2023, at 6:00 p.m.

Written materials distributed to the Planning Commission within 72 hours of the Planning Commission meeting are available for public inspection via the City's website and copies are available for public inspection beginning the next regular business day in the City Clerk's Office, 24300 Narbonne Avenue, Lomita.

In compliance with the Americans with Disabilities Act (ADA) if you need special assistance to participate in this meeting, please contact the office of the City Clerk at (310) 325-7110. Notification at least forty-eight (48) hours prior to the meeting will enable the City to make reasonable arrangements.

Any person may appeal all matters approved or denied by the Planning Commission to City Council within 30 days of receipt of notice of action by the applicant. Payment of an appeal fee is required. For further information, contact City Hall at (310) 325-7110.

I hereby certify under penalty of perjury under the laws of the State of California that the foregoing agenda was posted not less than 72 hours prior to the meeting at the following locations: Lomita City Hall, Lomita Park, and uploaded to the City of Lomita website http://www.lomita.com/cityhall/city_agendas/.

Dated Posted: June 7, 2023



Linda E. Abbott, CMC
Deputy City Clerk

**MINUTES OF THE
REGULAR MEETING
LOMITA PLANNING COMMISSION
MONDAY, MAY 8, 2023**

1. OPENING CEREMONIES

a. Call Meeting to Order

The regular meeting of the Lomita Planning Commission was called to order by Chair Cammarata at 6:05 p.m. on Monday, May 8, 2023, in the Council Chambers at Lomita City Hall, 24300 Narbonne Avenue, Lomita, California.

b. Flag Salute

Vice-Chair Graf led the salute to the flag.

c. Roll Call

PRESENT: Commissioners Dever, Sanchez, Santos, Stephens, Vice-Chair Graf, and Chair Cammarata

ABSENT: Steinbach

STAFF PRESENT: Community and Economic Director Rindge, Associate Planner MacMorran, Assistant Planner Barbero, Assistant City Attorney King, and Deputy City Clerk Abbott

2. ORAL COMMUNICATIONS

Chair Cammarata announced the time for public comments on Consent Agenda items or subjects other than those scheduled. There being no requests to speak, Chair Cammarata closed oral communications.

3. CONSENT AGENDA

a. APPROVAL OF MINUTES: April 10, 2023

RECOMMENDED ACTION: Approve minutes.

Commissioner Santos made a motion, seconded by Vice-Chair Graf, to approve the minutes.

MOTION CARRIED by the following vote:

AYES: Commissioners Dever, Sanchez, Santos, Stephens, Vice-Chair Graf, and Chair Cammarata

NOES: None

ABSENT: Steinbach

4. PUBLIC HEARINGS

- a. **CONDITIONAL USE PERMIT NO. 323 AT 2040 PACIFIC COAST HIGHWAY IN THE COMMERCIAL RETAIL (C-R) ZONE** (Applicant: James Shigeru Kemi, Fukanoya Japanese Sushi Restaurant, 2040 Pacific Coast Highway, Suite L, Lomita, CA 90717)

RECOMMENDED ACTION: Adopt a resolution approving Conditional Use Permit No. 323 to allow the existing restaurant at 2040 Pacific Coast Highway, Unit L to offer beer and wine for onsite consumption until 11:00 p.m. and exemption from the California Environment Quality Act (CEQA).

Commissioner Dever recused herself from discussion of this item due to the proximity of her home to the site, and left the dais and the Council Chambers.

Assistant Planner Barbero presented the staff report per the agenda material.

Brief discussion was held relative to business licensing and extended alcohol service hours.

Chair Cammarata opened the public hearing at 6:18 p.m.

George Kivett, Lomita resident, stated that sushi restaurants tend to have later operating hours, and suggested allowing the applicant to stay open until midnight.

There being no further requests to speak on this item, Chair Cammarata closed the public hearing at 6:20 p.m. and brought the item back for further discussion.

Brief discussion was held relative to the service of alcohol after food service ends during later hours.

Chair Cammarata invited further discussion or a motion.

Commissioner Sanchez made a motion, seconded by Commissioner Stephens, to adopt a resolution approving Conditional Use Permit No. 323 to allow the existing restaurant at 2040 Pacific Coast Highway, Unit L to offer beer and wine for onsite consumption until 10 p.m. Tuesday-Thursday, and 11:00 p.m. closure Friday and Saturday (closed Sunday and Monday) and exemption from the California Environment Quality Act (CEQA).

MOTION CARRIED by the following vote:

AYES: Commissioners Sanchez, Santos, Stephens, Vice-Chair Graf, and Chair Cammarata

NOES: None

ABSENT: Steinbach

RECUSED: Dever

Commissioner Dever returned to the Council Chambers and the dais at 6:37 p.m.

- b. **DISCUSSION AND CONSIDERATION OF ZONING TEXT AMENDMENT NO. 2023-06, AN ORDINANCE AMENDING LOMITA MUNICIPAL CODE TITLE XI (PLANNING AND ZONING) MODIFYING ACCESSORY DWELLING UNIT (ADU) REGULATIONS PER**

CALIFORNIA STATE MANDATE AND FINDING THE ACTION TO BE EXEMPT FROM THE CALIFORNIA ENVIRONMENTAL QUALITY ACT (Applicant: The City of Lomita)

RECOMMENDED ACTION: Adopt a resolution recommending City Council approval of Zoning Text Amendment 2023-06 to the City Council, subject to the findings and conditions and finding the request exempt from the California Environment Quality Act (CEQA).

Associate Planner MacMorran presented the staff report per the agenda material. She clarified that this ordinance is necessary for consistency, as it cleans up the City’s existing accessory dwelling unit regulations per recently adopted State mandates. This ordinance will align the City with State law, while keeping Lomita’s requirements, so there will be no need for frequent updates.

Brief discussion was held relative to maximum sizes and heights for ADUs.

Chair Cammarata opened the public hearing at 6:59 p.m. As there were no requests from the public to speak on this item, he closed the public hearing at 6:59 p.m., and brought the item back for further discussion or a motion.

Brief discussion was held relative to lenders and banks classifying properties containing an ADU as “two on a lot” and refusing to finance under some circumstances.

Vice-Chair Graf made a motion, seconded by Commissioner Santos, to adopt a resolution recommending City Council approval of Zoning Text Amendment 2023-06 to the City Council, subject to the findings and conditions and finding the request exempt from the California Environment Quality Act (CEQA).

MOTION CARRIED by the following vote:

- AYES: Commissioners Dever, Sanchez, Santos, Stephens, Vice-Chair Graf, and Chair Cammarata
- NOES: None
- ABSENT: Steinbach

5. SCHEDULED MATTERS

a. COMMUNICATIONS REGARDING CITY COUNCIL ACTIONS

Director Rindge stated that the quarterly update on City projects is available on the City’s website, and today was the kickoff for the General Plan Update. She stated that General Plan Advisory Committee (GPAC) applications are due May 24, 2023, and gave a summary of GPAC’s meeting schedule.

6. OTHER MATTERS

a. STAFF ANNOUNCEMENTS

None

b. PLANNING COMMISSIONER ITEMS

Commissioner Dever asked Assistant City Attorney King if he had found the answer to an election districts-related question from the last meeting. He stated that he would follow up with her ASAP. Commissioner Dever also asked the name of the new grocery store in the former Big Lots! location. Director Rindge stated that Grocery Outlet is taking over the space.

Commissioner Sanchez inquired as to who made the decision to drop the Economic Element from the General Plan Update. Director Rindge stated that it was not dropped; rather it can be tied into each of the other elements or kept on its own.

Chair Cammarata remarked on the May 7, 2023, *Los Angeles Times* article about hiring the right contractor when building an ADU. He also commented on seeing more graffiti of late in certain parts of the City. Director Rindge encouraged residents to submit public property graffiti abatement requests online.

In response to several comments, Director Rindge stated that staff will review Planning Commission's Rules and Procedures and recommend uniformity for all of the City's commissions.

7. COMMISSIONERS TO ATTEND CITY COUNCIL MEETINGS

Chair Cammarata will attend the Tuesday, June 6, 2023, City Council meeting; Commissioner Stephens will attend the Tuesday, June 20, 2023, City Council meeting.

8. ADJOURNMENT

As there was no further business to discuss, Chair Cammarata adjourned the meeting at 7:17 p.m.

Linda E. Abbott, CMC
Deputy City Clerk



CITY OF LOMITA PLANNING COMMISSION REPORT

TO: Planning Commission **Item No. PH 4a**

FROM: Erika Barbero, Assistant Planner

MEETING DATE: June 12, 2023

SUBJECT: Amendment to Site Plan Review No. 1049; Zone Variance No. 246; Minor Conditional Use Permit No. 5 for 2 Unpermitted Units Totaling 1,658 ft², the Unpermitted Expansion of Units 204, 205, and 206, a 256-Square-Foot Patio, to Allow up to 50 Percent of the Ground Floor to be Occupied by a Combination of Uses Identified in Lomita Municipal Code Sec. 11-1.48.02(B); Variance to Remove the Loading Zone Requirement in Order to Add Regular Parking Spaces; and for a Shared Parking Arrangement at 25820 Lucille Avenue within the Commercial Retail (C-R) Zone; and Finding the Action to be Exempt from the California Environmental Quality Act

RECOMMENDATION

Adopt a resolution amending Site Plan Review No. 1049 to legalize unpermitted construction and to allow a combination of uses identified in LMC Sec. 11-1.48.02(B) up to 50 percent, approve a Variance to allow the removal of the loading zone for additional parking, approve a minor conditional use permit for shared parking at 25820 Lucille Avenue in the Commercial Retail (C-R) Zone, and confirm the categorical exemption from the California Environmental Quality Act (CEQA).

BACKGROUND

The subject property, Peninsula Plaza, was approved in September 2005 under Site Plan Review (SPR) No. 1049 with modifications to the original request. The entitlement permitted the construction of a two-story, 17,390 ft² multi-tenant, retail/office building with a total of 12 leasable units, 7 on the first floor and 5 on the second floor. The project originally supplied 58 parking spaces at a parking ratio of 1 space for every 300 ft² of building area. The original site plan contained 16 surface parking spaces, a surface loading zone in front of Unit 107, and 42 subterranean parking spaces.

In 2014, the Planning Commission approved the removal of tiger-teeth traffic control devices from the Lucille Avenue entrance and restricted traffic to right-turn-only.

In October 2021, the Planning Commission approved Site Plan Review No. 1212 to allow for a combination of uses specified in Lomita Municipal Code 11-1.48.03(B) to occupy 46% of the ground floor area. However, the approval was rescinded because the plans furnished were inconsistent with the approved site plan and conditions. The applicant, Jeff S. Coffman, filed on behalf of the property owner, David Nosrati / Maxnet Commercial LLC, applications for several entitlements to bring the site into compliance with the Lomita Municipal Code.

Existing Conditions

The subject site is located on the southeast corner of Lucille Avenue and Pacific Coast Highway (PCH). The total area of the property is 32,306 ft², containing a two-story, 20,443 ft² multi-tenant building, 14,406 ft² of subterranean parking to accommodate 42 vehicles and 29 surface parking spaces. The building maintains the original 12 leasable units, however, the sizes of 3 of the second-floor units are larger than originally approved for, 2 unpermitted units, an unpermitted storage unit, and an unpermitted patio along Pacific Coast Highway. The site offers vehicle circulation via a two-way driveway located off Pacific Coast Highway and a second driveway off Lucille Avenue. The uses on the subject property vary between service, medical office, and educational-based uses.

ANALYSIS

General Plan

The land use designation for the property is Commercial and allows for a floor area ratio of 1.0 to 1.0. The proposed 20,443 ft² building sits on a 32,306 ft² lot and has a F.A.R. of less than 1.0. The commercial project is consistent with the Land Use Element's designation.

Zoning Review

The site is zoned C-R (Commercial Retail) which was established to provide regional retail sale needs. This zone provides for retail activities and limited-service establishments.

Adjacent Zoning and Land Uses

<i>Direction</i>	<i>Land Use</i>	<i>Zoning</i>
North	Commercial	Commercial Retail (C-R)
South	Residential	Single-Family Residential (R-1)
West	Commercial	Commercial Retail (C-R)
East	Commercial	Commercial Retail (C-R)

Environmental Determination

Staff reviewed the proposal and finds it exempt from the provisions of the California Environmental Quality Act (CEQA) as it is an expansion of existing use and meets categorical exemptions under Section 15301(a), Existing Facilities and Section 15332, In-Fill Development.

Site Plan Review No. 1049 Amendment

- legalize creation of Unit 108 (902 ft²) and Unit 201 (756 ft²)
- legalize the expansions of Units 204, 205, and 206
- legalize a 256 ft² patio adjacent to Unit 101 along Pacific Coast Highway
- increase the percentage of ground floor service uses allowed per Sec. 11-48.03(B) to the maximum allowed by Sec. 11-48.03(B) at 50%

An increase in service uses is appropriate for the ground floor due to the building's orientation to Lucille Avenue (a local noncommercial street). The lot shape, which is narrower along PCH and deeper along Lucille Avenue, predicates the building orientation. As such, the building has limited exposure to PCH. Only Unit 101, Unit 102, proposed Unit 201, and Unit 202 are reasonably visible from PCH. Most of the businesses may only be seen from Lucille Avenue, and visibility is an important site condition for non-service-based stores. In fact, the official building address is 25820 Lucille Avenue, instead of 2244 PCH.

A June 7, 2023 site visit for presentation photos revealed that the current building does not conform to the plans submitted (Attachment 7). This site plan review amendment shall only regard the 4 items specifically listed above, as reflected on the submitted plans and the proposed resolution (Attachment 1).

Site Plan Review Findings Summary

Staff reviewed the project as proposed (Attachment 7) in accordance with Sec. 11-1.70.07. (Site Plan Review) of the Lomita Municipal Code and deems the project consistent with the required findings (see Attachment 1).

Minor Conditional Use Permit No. 5

Peninsula Plaza currently provides a total of 71 parking spaces; however, the site originally provided 58 spaces and was parked at one parking space for every 300 ft², which is the parking ratio for office and retail uses. By removing the loading zone, they increased the parking supply, and can accommodate uses that have a greater parking demand; however, the site does not have enough space to meet the Code's commercial mixed-use standard of 1 space for every 200 ft² of gross floor area.

This applicant supplied a site-specific shared parking analysis based on the existing tenant types and varying peak demand times. The Shared Parking Analysis authored by

RK Engineering Group (Attachment 9) utilizes the Urban Land Institute's (ULI) parking demand profiles to calculate the parking demand for the existing and proposed uses.

The study predicts the peak demand on weekdays to be at 1:00 p.m., providing a surplus of 4 spaces. This excess capacity will allow a user with a parking demand more intense than office/retail to lease space. It is likely that given the unit sizes, only one unit would be allowed to have a tenant that required more parking, such as a medical office. On the weekend, the peak demand will occur at 1:00 pm with the demand for 60 spaces with a surplus of 11 spaces. This is due to the closing of offices on the weekends. The analysis provides a table of findings which can be found in the Tables and Graphs section of the Analysis.

Zone Variance No. 246

Loading zones are required for any property with more than 7,000 ft² of building space (LMC Sec. 11-1.66.04). The removal of the loading zone, removal of some excess landscaping, and changes to the subterranean parking area created 13 additional parking spaces. Loading zones are critical for large properties as deliveries are a certainty, but the delivery truck size varies based on the type of business. For example, a big box retailer or grocery store cannot exist without regular semi-truck deliveries; however, it would be unlikely for offices, small retailers, nail salons, and ophthalmologists to receive a delivery from a large delivery truck. Online images from 2014 do not show a striped loading zone; they do, however, show the current parking layout.

Site Plan Review Findings Summary

Staff reviewed the project in accordance with Sec. 11-1.70.10 (Variance) of the Lomita Municipal Code and deems the project consistent with the required findings (see Attachment 1).

PUBLIC NOTICE

On June 2, 2023, staff mailed notices of this hearing to property owners within 300' of the subject property and posted on the Lomita City web page, at Lomita City Hall, and at Lomita Park.

OPTIONS

1. Approve the attached resolutions as recommended.
2. Approve the attached resolutions with amended conditions.
3. Deny the application.
4. Provide staff with further direction.

ATTACHMENT

1. Draft Resolution
2. Vicinity Map

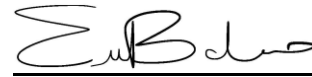
3. Zoning Map
4. General Plan Map
5. Aerial Photograph
6. Notice of Exemption
7. Project Plans
8. Site Plan No. 1049
9. Parking Analysis

Recommended by:



Brianna Rindge, AICP
Community & Economic Development Director

Prepared by:



Erika Barbero
Assistant Planner

**PLANNING COMMISSION
RESOLUTION NO. PC 2023-14
AMENDMENT TO SITE PLAN REVIEW NO. 1049, ZONE VARIANCE
NO. 246, AND MINOR CONDITIONAL USE PERMIT NO. 5 FOR 25820
LUCILLE AVENUE WITHIN THE COMMERCIAL RETAIL (C-R) ZONE
APN: 7553-002-035**

A RESOLUTION TO APPROVE AMENDMENT TO SITE PLAN REVIEW NO. 1049, ZONE VARIANCE NO. 246, AND MINOR CONDITIONAL USE PERMIT NO. 5: A REQUEST FOR A SITE PLAN AMENDMENT FOR 2 UNPERMITTED UNITS TOTALING 1,658 SQUARE FEET, THE UNPERMITTED EXPANSION OF UNITS 204, 205, AND 206, A 256-SQUARE-FOOT PATIO, AND TO ALLOW FOR UP TO 50 PERCENT OF SPACE FOR A COMBINATION OF USES IDENTIFIED IN LOMITA MUNICIPAL CODE SEC. 11-1.48.02(B); A REQUEST FOR VARIANCE TO REMOVE THE REQUIREMENT FOR A LOADING ZONE IN ORDER TO ADD REGULAR PARKING SPACES; AND A REQUEST FOR A MINOR CONDITIONAL USE PERMIT FOR A SHARED PARKING ARRANGEMENT AT 25820 LUCILLE AVENUE WITHIN THE COMMERCIAL RETAIL (C-R) ZONE. FILED BY JEFF S. COFFMAN ON BEHALF OF PROPERTY OWNER DAVID NOSRATI / MAXNET COMMERCIAL LLC (APPLICANT)

THE PLANNING COMMISSION OF THE CITY OF LOMITA DOES HEREBY FIND, ORDER, AND RESOLVE AS FOLLOWS:

Section 1. Recitals

- A. The City of Lomita received an application to request for a Site Plan Amendment for 2 unpermitted units totaling 1,658 square feet, the unpermitted expansion of units 204, 205, and 206, a 256-square-foot patio, and to allow for up to 50 percent of space for a combination of uses identified in Lomita Municipal Code Sec. 11-1.48.02(B); a request for Variance to remove the loading zone requirement in order to add regular parking spaces; and a request for a Minor Conditional Use Permit for a shared parking arrangement at 25820 Lucille Avenue within the Commercial Retail (C-R) zone, filed by Jeff S. Coffman on behalf of property owner David Nosrati / Maxnet Commercial LLC (applicant).
- B. On June 12, 2023, the Planning Commission of the City of Lomita held a duly noticed public hearing and accepted public testimony for an application.
- C. The project is categorically exempt pursuant to Section 15301 (Existing Facilities) of the California Environmental Quality Act guidelines. The project consists of the legalization of unpermitted construction within the existing building approved in 2005. The proposed project legalizes a total of 2,761 ft² square feet which is less than 50% of the total floor area of the existing building and less than 10,000 square feet.

- D. The Planning Commission finds that the Applicants agree with the necessity of and accept all elements, requirements, and conditions of this resolution as being a reasonable manner of preserving, protecting, providing for, and fostering, the health, safety, and welfare of citizens in general including those who access the site.

Section 2. Findings

Amendment of Site Plan Review No. 1049 is subject to the findings contained in Lomita Municipal Code Sections 11-1.70.07 (Site Plan Review). The required findings below are shown in italicized type and the reason(s) the project is consistent is shown in regular type. Planning Commission finds, after due study and deliberation, that the following circumstances exist:

1. The Site Plan complies with all applicable provisions of this Title.

The structure was originally approved in 2005 and was designed to accommodate office and retail use. The applicant modified the building to expand the existing land use. The subject property is already substantially developed, with no additional building coverage proposed beyond the state of the site today. The request includes the legalization of unpermitted retail units, an unpermitted storage unit, a patio, and unpermitted unit expansions on the second floor. The applicant also requests an increase of allowable uses identified in Lomita Municipal Code Section 11-1.66.03 (B). The Planning Commission may approve additional commercial uses beyond 30 percent, no more than 50 percent, on the ground floor, under Article 70. Subject to compliance with the Resolution's Conditions of Approval, the project complies with all applicable provisions in Title 11 – Planning and Zoning.

2. The site is suitable for the particular use or development intended, and the total development, including the application of prescribed development standards, is arranged as to avoid traffic congestion, will not adversely affect public health, safety, and general welfare, will not have adverse effects on neighboring property and is consistent with all elements of the General Plan.

The site's original entitlement and design established that office and retail uses were the target for this location. The current tenants at the site indicate that the unpermitted site modifications were intended to continue those uses. Such uses are consistent with the General Plan. Due to the minor nature of the unit expansions, allowing a combination of uses identified in Lomita Municipal Code Section 11-1.66.03 (B) at this location is compatible with the land use and will not have any negative impacts on the neighboring properties and more than likely will create economic opportunities for the City and promote walkability for residents in the area.

Approval of the request by the Planning Commission will eliminate negative impacts on public health and safety by requiring the business owner to gain approval through Los Angeles County Building and Safety and obtain a Certificate of Occupancy before leasing to new businesses. The site maintains substantial parking with two access points, one off Pacific Coast Highway and the second off Lucille Avenue.

The percentage of the building fronting Pacific Coast Highway is significantly lower than other sites, in part due to the lot's shape. The Pacific Coast Highway frontage is less than half the lot's depth, and approximately half the width is occupied by a driveway aisle, surface parking, and landscaping. These conditions make the building less suitable for retail uses, and more suitable for service and office uses. The owner reports limited success in finding a restaurant and retail tenants. By allowing up to 50% of the ground floor to be occupied by uses specified in Sec. 11-1.48.02(B) it will improve occupancy and property maintenance.

3. The development design is suitable and functional. This requirement shall not be interpreted to require a particular style or type or architecture.

The exterior façade of the existing commercial building will remain unchanged apart from the patio. The building design remains suitable and functional for the neighborhood.

The project is subject to the Minor Conditional Use Permit findings contained in Lomita Municipal Code Section 11-1.66.10 (Minor Conditional Use Permit). The required findings below are shown in *italicized* type and the reason(s) the project is consistent is shown in regular type. Planning Commission finds, after due study and deliberation, that the following circumstances exist:

1. The peak hour parking demand from all uses does not coincide and/or the uses are such that the hours of operation are different for various portions of the business.

The tenants that utilize this building are service-based and retail-based uses with varying hours of operation. Per the Shared Analysis report authored by Consulting firm RK Engineering Group Inc. (Attachment 8 to the staff report), it was found that the peak hour parking demand of all existing uses does not create parking conflicts and yields a surplus of parking.

2. The quantity, circulation, and location of parking provided will equal or exceed the level that can be expected if shared or off-site parking is not provided.

The 71 provided spaces satisfy parking demand at 1:00 p.m. during both weekday and weekend peak demand times per the Shared Analysis report (Attachment 8). Per the report, the site yields a surplus of 4 spaces during weekday hours and a surplus of 11 spaces during weekend hours. The site offers proper circulation via the exit/entry points located on both Lucille Avenue and Pacific Coast Highway.

3. The adjacent or nearby properties will not be adversely affected relative to parking.

The plaza sits along a major commercial corridor in the city and offers subterranean parking. Since the analysis projects a surplus of spaces, adjacent properties should not be affected by parking.

4. The proposed traffic circulation will not be detrimental to the health, safety, and welfare of residents residing or working in or adjacent to the neighborhood.

The interior circulation facilitates flow to and from the property. Access to the site occurs at either one of two driveways: off Pacific Coast Highway to the north and Lucille Avenue to the west.

5. There is clear and convincing evidence that the parking demand will be less than the requirement in Section 11-1.66.03.

The Shared Analysis report demonstrates that there will be a surplus of parking spaces during peak hours of 1:00 p.m. on both weekdays and weekends, therefore the proposal yields sufficient parking for the property.

6. That the probable long-term occupancy of the building or structure, based on its design, will not generate additional parking demand.

The Commercial Retail (C-R) Zone allows for retail, service, and professional office uses. Since the property maintains a reported surplus of parking spaces, there exists flexibility for the property to accommodate additional parking for another office, retail, or service use.

The project is subject to the Variance findings contained in Lomita Municipal Code Section 11-1.70.10 (Variance). The required findings below are shown in *italicized* type and the reason(s) the project is consistent is shown in regular type. Planning Commission finds, after due study and deliberation, that the following circumstances exist:

- 1. There are exceptional or extraordinary circumstances or conditions applicable to the property involved, including size, shape, topography, location, or surroundings, which do not generally apply to the surrounding properties in the same zone; and which deny the owner privileges enjoyed by other property owners in the vicinity and within the same zoning district.*

This building's design and orientation were driven by the lot's uncommon shape and orientation for a property on our largest commercial corridor. The lot is twice as deep as it is wide and a two-way driveway aisle and parking occupy more than 50% of the Pacific Coast Highway frontage. The property contains about 13,000 ft² across 14 units ranging in size from 756 ft² to 2,114 ft². Typically, units of this size tend to be free-standing or in smaller shopping centers with lower service capacities. Providing a loading zone creates undue hardship as the shopping center was not designed to host high-volume retail sales that require large delivery vehicles or consistent offloading of goods. It is expected that smaller vehicles which more fit a regular parking space would service the site's businesses due to the size and nature of the units, which is restricted by the parking minimum permitted through the Minor Conditional Use Permit.

2. *Because of the circumstances or conditions, the variance is necessary for the preservation and enjoyment of a substantial property right possessed by other property similarly situated but which is denied to the property in question.*

A variance is necessary in order to provide sufficient parking to enable a variety of tenants to occupy the building and realize floor area. Based on a floor area ratio of 1.0 to 1.0, this property could possess over 33,000 ft²; however, the building contains a little over 20,000 ft². The presence of the loading area precludes the use of almost 70 linear feet of space along the westerly side. To cope with its shape challenges, this building, unlike other commercial properties in Lomita, built a subterranean garage so that it could achieve an improved floor area ratio.

3. *The granting of the variance will not be materially detrimental to the public welfare or injurious to the adjacent property.*

Granting the variance will not be detrimental to the public welfare or adjacent properties and will enhance public safety as it will allow for more parking spaces for patrons and eliminate parking or traffic issues on the adjacent residential street.

4. *The variance will not be materially detrimental to public welfare.*

Based on the unit sizes, it is unlikely that semi-trucks or large delivery trucks will deliver to this property. Evidence indicates that the site was built in 2014 without the loading area and instead with the additional parking, which is of value to the nearby Lucille Avenue residents. Staff did not find any records of complaints pertaining to delivery trucks.

5. *The granting of the variance is consistent with the General Plan.*

Granting the Variance is consistent with the General Plan because allowing the removal of the loading zone will eliminate unintended traffic congestion by improving traffic flow and it will boost economic growth within the city.

Section 3. Based on the above findings, the Planning Commission of the City of Lomita hereby approves the following, subject to the conditions below:

- a. The amendment to Site Plan Review No. 1049 to legalize the creation of Unit 108 (902 square feet) and Unit 201 (756 square feet); legalize the expansions of Units 204, 205, and 206; legalize a 256 square-foot patio adjacent to Unit 101 along Pacific Coast Highway and to increase the percentage of ground floor service uses allowed per Lomita Municipal Code section 11-48.03(B) to the maximum allowed at 50%;
- b. Zone Variance No. 246 to remove the requirement for a loading zone so that parking spaces may be added; and

- c. Minor Conditional Use Permit No. 5 for a shared parking arrangement at 25820 Lucille Avenue providing a minimum of 67 standard parking spaces.

GENERAL PROJECT CONDITIONS

1. The project shall conform to the plans as provided as Attachment 7 to the staff report, except as otherwise specified in these conditions, or unless a minor modification to the plans is approved by the Community and Economic Development Director or a major modification to the plans is approved by the Planning Commission.
2. A building permit shall be obtained for all unpermitted construction that was found prior to the listed entitlement applications.
3. This permit is granted for the Applicant's application received on April 10, 2023, on file with the Planning Division, and may not be transferred from one property to another.
4. By commencing any activity related to the project or using any structure authorized by this permit, Applicant accepts all of the conditions and obligations imposed by this permit and waives any challenge to the validity of the conditions and obligations stated therein.
5. This permit shall not be effective for any purpose until a duly authorized representative of the owner of the property has filed with the Department of Community Development a notarized affidavit accepting all the conditions of this permit. This affidavit, or a copy of this resolution, shall be recorded with the County Recorder and is binding on successors. If the Applicant is a corporation, then an officer of the corporation shall sign the acceptance affidavit.
6. This permit shall automatically be null and void two years from the date of issuance unless a business license has been issued by the City of Lomita. A request for a one-year extension may be considered by the Planning Commission. No extension shall be considered unless requested prior to the expiration date.
7. In the event of a disagreement in the interpretation and/or application of these conditions, the issue shall be referred back to the Planning Commission.
8. It is hereby declared to be the intent that if any provision of this permit is held or declared invalid, the permit shall be void and the privileges granted hereunder shall lapse.
9. All outstanding fees owed to the City, including staff time spent processing this application, shall be paid in full.

PLANNING CONDITIONS

10. Applicant shall comply with all previous requirements and conditions of Site Plan No. 1049 except as modified herein.
11. This approval refers to the plans as presented by Attachment 7 to the staff report; businesses may only operate strictly within the units labeled Suite 101 (including the adjacent 256-square-foot patio), Suite 102, Suite 103, Suite 104, Suite 105, Suite 106, Suite 107, Suite 108, Suite 201, Suite 202, Suite 203, Suite 204, Suite 205, and Suite 206. The room labeled "Storage Only Non-Leasable" on the approved plans shall remain strictly as storage only and may not be used as office, breakroom, meeting space, or similar.
12. Prior to the use of all unpermitted units, plumbing, storage facilities, and the patio in the building, the property owner shall obtain all required building permits and a certificate of occupancy, including payment of per-unit development fees.
13. Specifically, each unit may maintain one bathroom within its footprint once permits and certificate of occupancy have been obtained.
14. Unit 107 shall open to the second story.
15. Uses identified in LMC Sec. 11-1.48.02(B) are permitted within up to 50 percent of the floor area on the ground floor level.
16. The Planning Commission shall review and approve any combining of a unit or units with other units.
17. Specifically, the site shall adhere to Section 11-1.66.05 ("General provisions") of Article 66 ("Off-Street Parking, Storage and Loading") of Title XI of the Lomita Municipal Code, unless otherwise approved by Planning Commission through a subsequent application.

INDEMNIFICATION

Applicant agrees, as a condition of adoption of this resolution, at Applicant's own expense, to indemnify, defend and hold harmless the City and its agents, officers and employees from and against any claim, action or proceeding to attack, review, set aside, void or annul the approval of the resolution or any condition attached thereto or any proceedings, acts or determinations taken, done or made prior to the approval of such resolution that were part of the approval process. Applicant's commencement of construction or operations pursuant to the resolution shall be deemed to be an acceptance of all conditions thereof.

COMPLIANCE

If Applicant, owner, or tenant fails to comply with any of the conditions of this permit, the Applicant, owner or tenant shall be subject to a civil fine pursuant to the City of Lomita Municipal Code.

If any of the conditions of the Minor Conditional Use Permit are not complied with or upon notice of a violation by the Code Enforcement Division, the Planning Commission, after written notice to the applicant and a noticed public hearing, may in addition to revoking the permit, amend, delete, or add conditions to this permit at a subsequent public hearing.

PASSED and ADOPTED by the Planning Commission of the City of Lomita on this 12th day of June 2023 by the following vote:

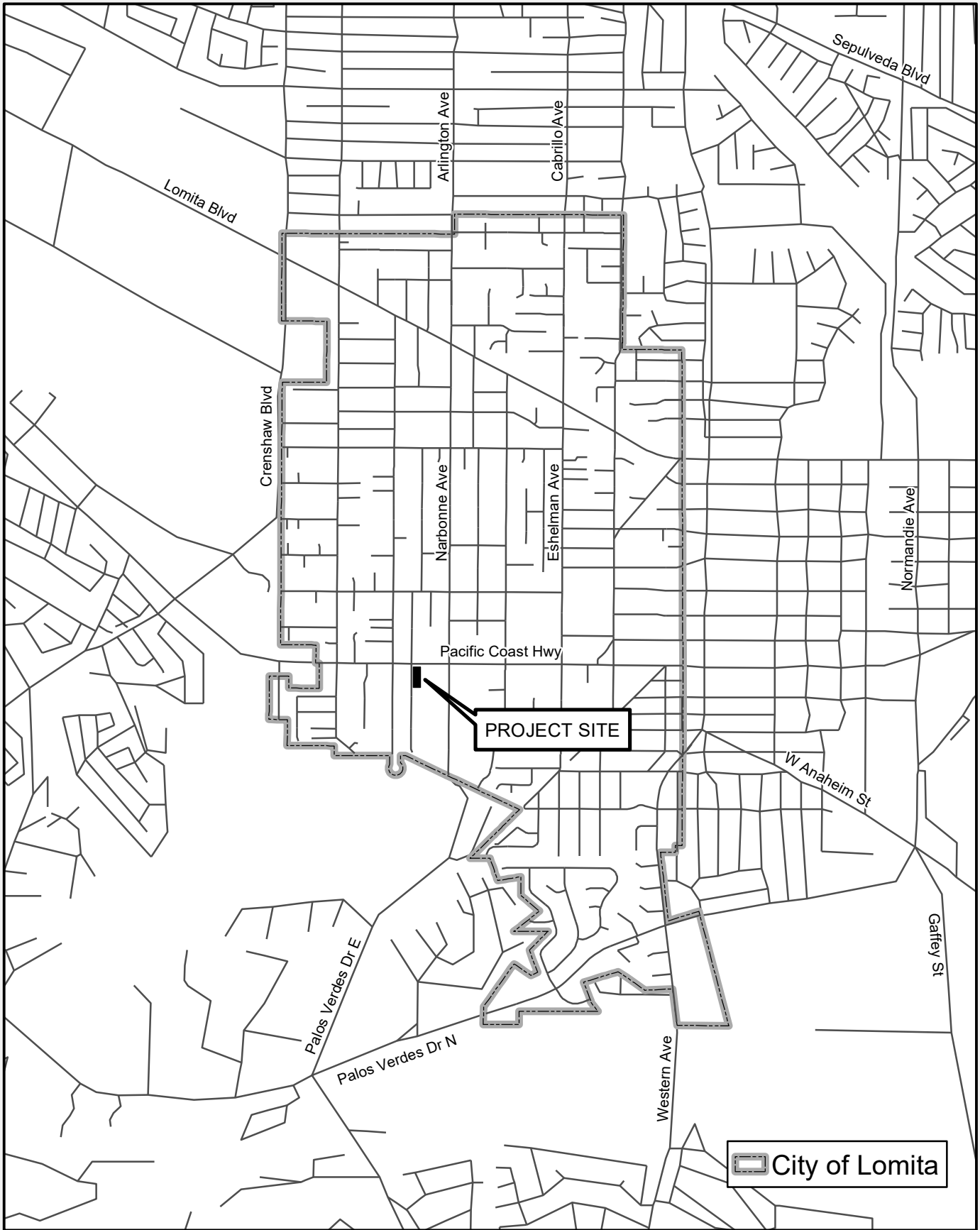
AYES: Commissioners:
NOES: Commissioners:
ABSENT: Commissioners:
RECUSE: Commissioners:

Steven Cammarata, Chairperson

ATTEST: _____
Brianna Rindge, AICP
Community and Economic Development Director

Within 30 days of the date of this decision for an exception, permit, change of zone, or other approval, or by the person the revocation of whose permit, exception, change of zone, or other approval is under consideration, of notice of the action of, or failure to act by, the Commission, any person dissatisfied with the action of, or the failure to act by, the Commission may file with the City Clerk an appeal from such action upon depositing with said Clerk an amount specified by resolution of the City Council.

Any action to challenge the final decision of the City made as a result of the public hearing on this application must be filed within the time limits set forth in Code of Civil Procedure Section 1094.6.

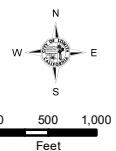


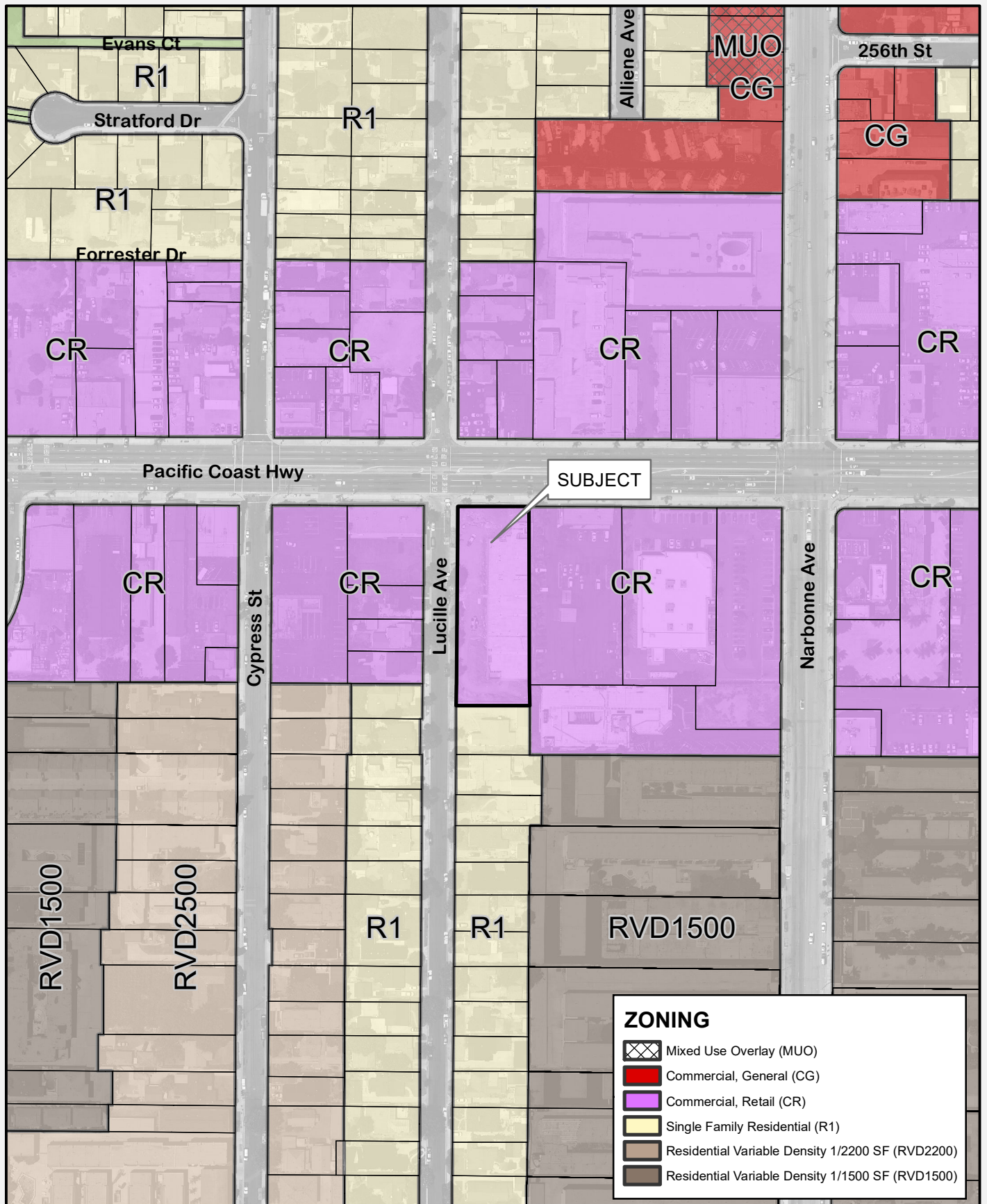
**Amendment to Site Plan Review
No. 1049; Zone Variance No. 246;
Minor Conditional Use Permit No. 5**







25820 Lucille Avenue

Community Development
June 2023

Source: Lomita GIS Data Layers, TIGER Files
GIS/Apps/2244PCH_4vty.mxd





ZONING	
	Mixed Use Overlay (MUO)
	Commercial, General (CG)
	Commercial, Retail (CR)
	Single Family Residential (R1)
	Residential Variable Density 1/2200 SF (RVD2200)
	Residential Variable Density 1/1500 SF (RVD1500)

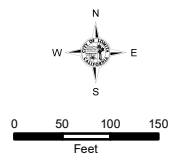


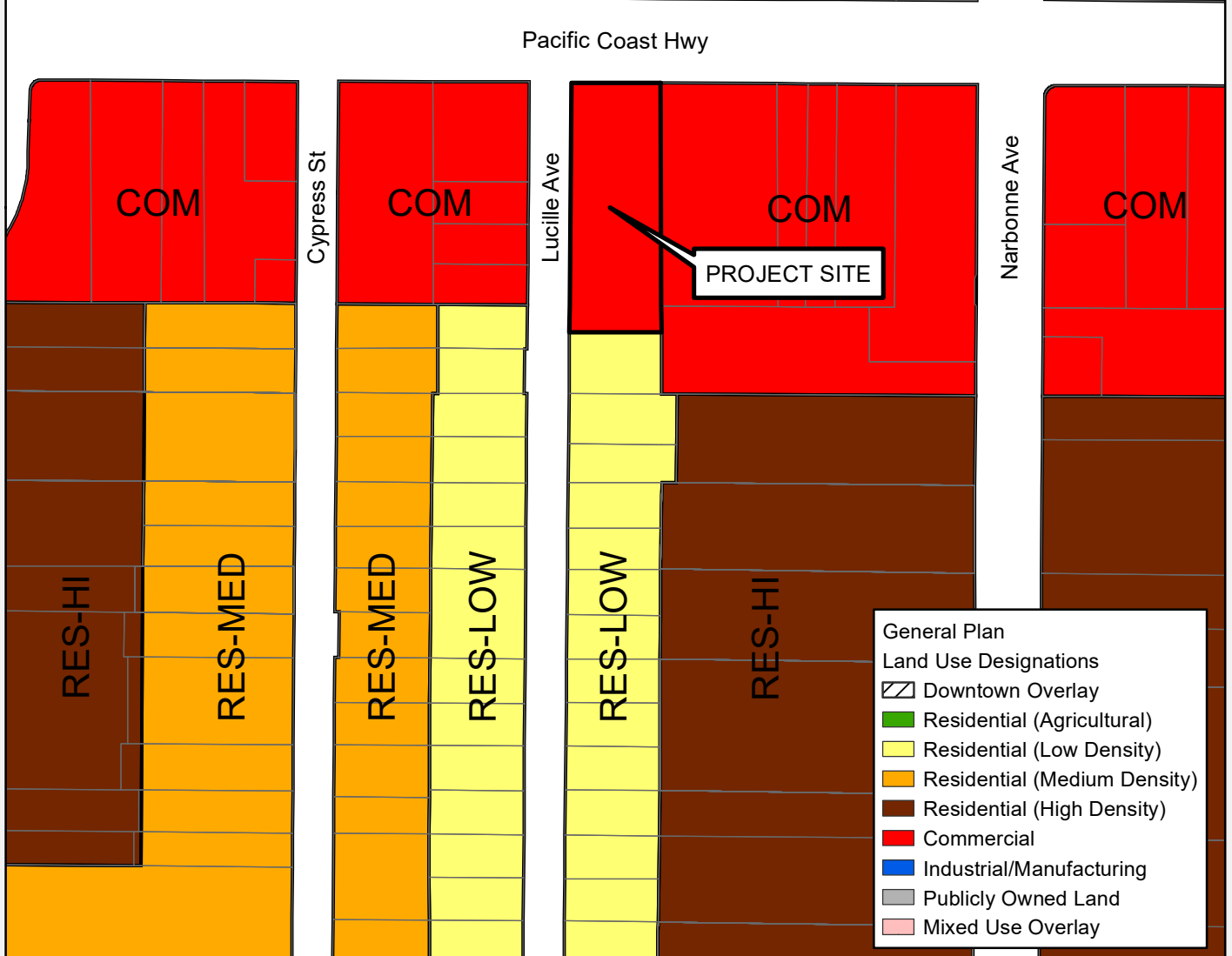
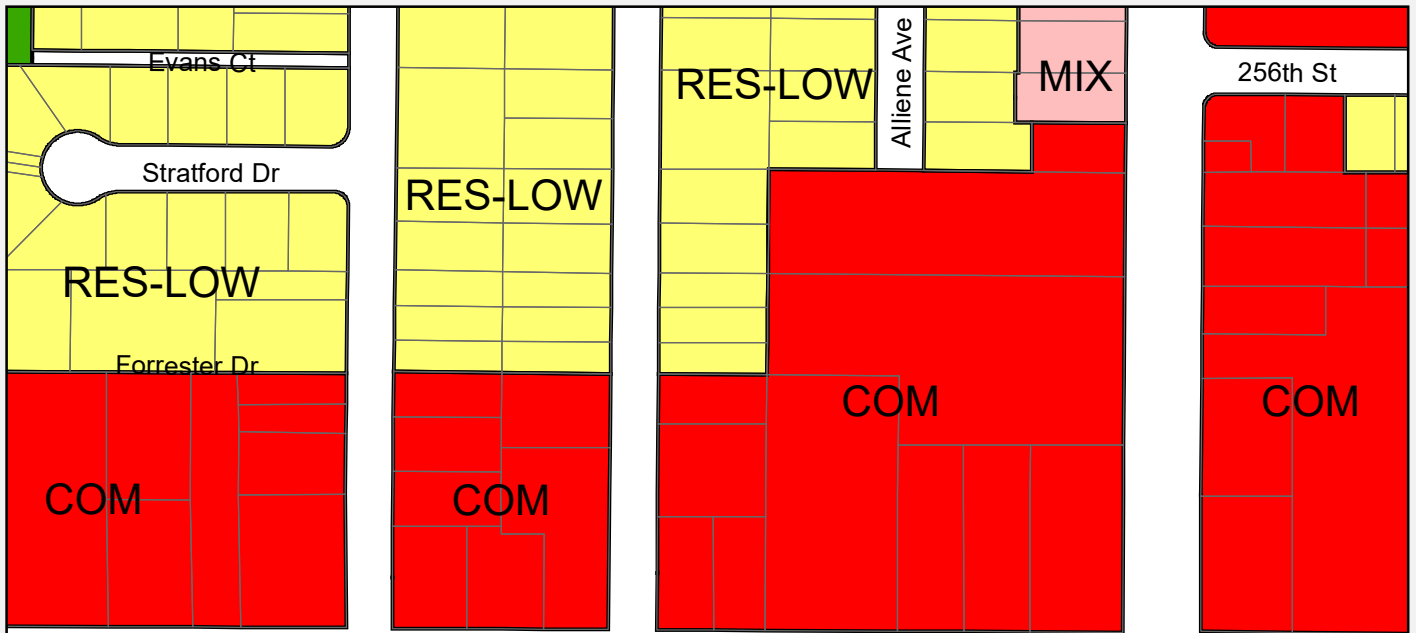
**Amendment to Site Plan Review
No. 1049; Zone Variance No. 246;
Minor Conditional Use Permit No. 5**

25820 Lucille Avenue

Community Development
June 2023

Source: Lomita GIS Data Layers
/Apps/2244PCH_4zon.mxd





General Plan
Land Use Designations

- Downtown Overlay
- Residential (Agricultural)
- Residential (Low Density)
- Residential (Medium Density)
- Residential (High Density)
- Commercial
- Industrial/Manufacturing
- Publicly Owned Land
- Mixed Use Overlay

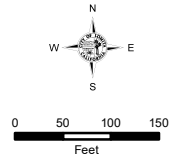


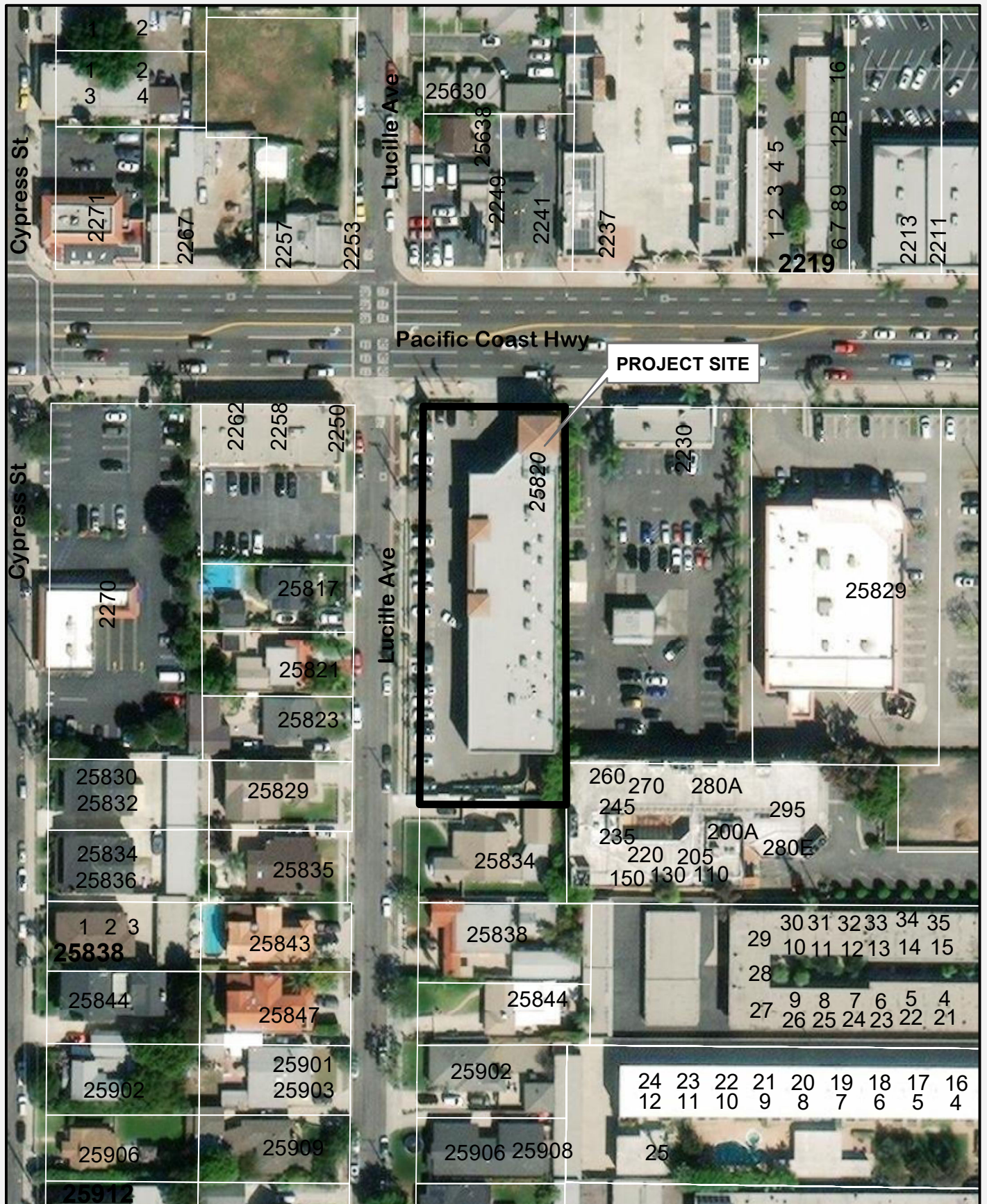
**Amendment to Site Plan Review
 No. 1049; Zone Variance No. 246;
 Minor Conditional Use Permit No. 5**

25820 Lucille Avenue

Community Development
 June 2023

Source: Lomita GIS Data Layers
 GIS/Apps/2244PCH_Agp.mxd



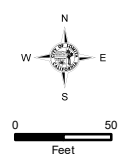


Amendment to Site Plan Review No. 1049; Zone Variance No. 246; Minor Conditional Use Permit No. 5

25820 Lucille Avenue

Community Development
June 2023

Source: Lomita GIS Data Layers
/Apps/224PCH_4aer.mxd





Community Development Department
 Planning Division
 24300 Narbonne Avenue
 Lomita, CA 90717
 310/325-7110
 FAX 310/325-4024

NOTICE OF EXEMPTION

Project Title: Amendment to Site Plan Review No. 1049; Zone Variance No. 246; Minor Conditional Use Permit No. 5

Project Description: A request for a Site Plan Amendment For 2 unpermitted units totaling 1,658 square feet, the unpermitted expansion of units 204, 205, & 206, a 256-square-foot patio, and to allow for up to 50 percent of space for a combination of uses identified in Lomita Municipal Code Sec. 11-1.48.02(B); A request for Variance to remove the loading zone requirement in order to add regular parking spaces; and a request for a Minor Conditional Use Permit for a shared parking arrangement at 25820 Lucille Avenue within the Commercial Retail (C-R) Zone.

Finding:

The Planning Division of the Community Development Department of the City of Lomita has reviewed the proposed project and found it to be exempt from the provisions of the California Environmental Quality Act (CEQA).

- Ministerial Project
- Categorical Exemption (CEQA Guidelines, Section 15301(a) (Existing Facilities) & Section 15332 (In-Fill Development))
- Statutory Exemption
- Emergency Project
- Quick Disapproval [CEQA Guidelines, Section 15270]
- No Possibility of Significant Effect [CEQA Guidelines, Section 15061(b)(3)]

Supporting Reasons: In accordance Pursuant to Section 15301 (Existing Facilities) of the CEQA guidelines, a project that consists of permitting or licensing involving negligible expansion of use may be found to be exempt from the requirements of CEQA. In addition, Section 15332 (In-Fill Development) exempts a project that is consistent with the general plan and zoning designation, is on an urban site less than five acres, is not a special habitat, does not create significant traffic, noise, air quality or water quality issues, and is adequately served by all utilities and public services. The proposed minor conditional use permit for shared parking is a permitting activity and intensification of an existing restaurant use meets the criteria for an infill development. Therefore, the Planning Commission has determined that there is no substantial evidence that the project may have a significant effect on the environment.

Date

Erika Barbero
Assistant Planner

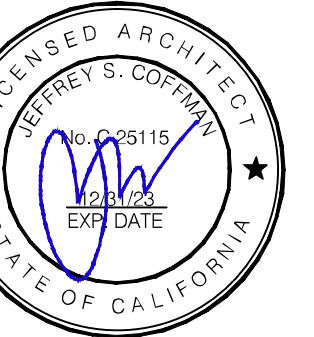
APPLICATION FOR APPROVAL OF PARKING ANALYSIS TO SERVE EXISTING TWO STORY RETAIL/OFFICE BUILDING

FOR:

PENINSULA PLAZA 25820 LUCILLE AVENUE, LOMITA, CA 90717

JEFF S. COFFMAN ARCHITECT
architecture
planning
research

2000 e chapman avenue
suite 202
fullerton, ca 92831
714.738.7045 p:



APPLICATION FOR APPROVAL OF PARKING ANALYSIS TO SERVE EXISTING TWO STORY RETAIL/OFFICE BUILDING

FOR:
PENINSULA PLAZA

25820 LUCILLE AVE
LOMITA, CA 90717

REVISIONS

COMMENTS

- 10/27/22 - ISSUED FOR PLANNING REVIEW
- 11/17/22 - ISSUED FOR PLANNING REVIEW
- 12/07/22 - ISSUED FOR PLANNING REVIEW
- 02/24/23 - ISSUED FOR PLANNING REVIEW
- 04/10/23 - SUBMIT PLANNING APPLICATIONS
- 05/23/23 - PLANNING COMMENTS

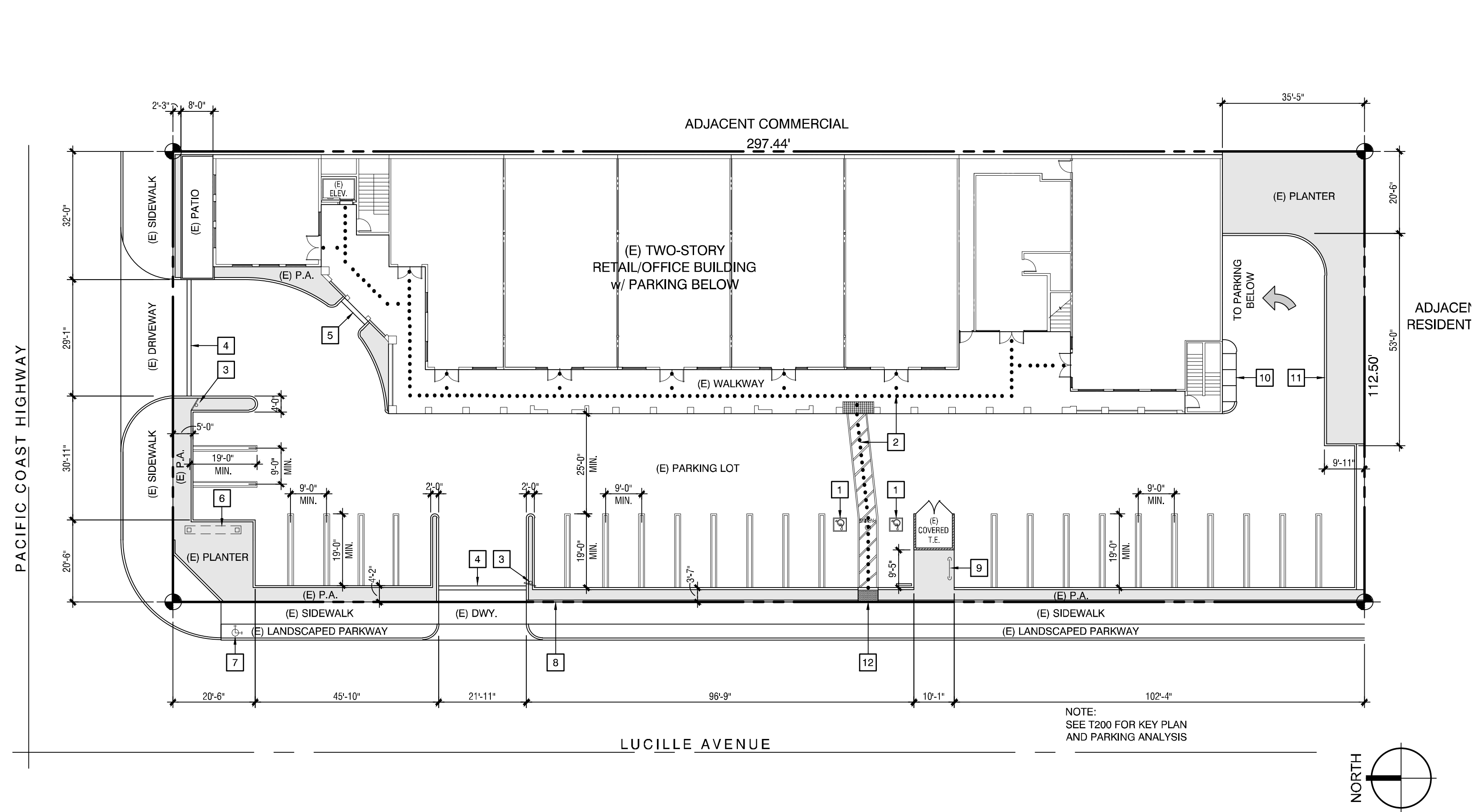
AS INSTRUMENTS OF SERVICE, ALL DESIGNS, IDEAS AND INFORMATION SHOWN ON THESE DRAWINGS ARE AND SHALL REMAIN THE PROPERTY OF JEFF S. COFFMAN ARCHITECT. NO PART THEREOF SHALL BE COPIED, DISCLOSED TO OTHERS OR USED IN CONNECTION WITH ANY WORK OR PROJECT OTHER THAN THE SPECIFIC PROJECT FOR WHICH THEY HAVE BEEN PREPARED AND DEVELOPED, WITHOUT THE WRITTEN CONSENT OF JEFF S. COFFMAN ARCHITECT. RECEIPT OF AND/OR VISUAL CONTACT WITH THESE DRAWINGS SHALL CONSTITUTE CONCLUSIVE EVIDENCE OF ACCEPTANCE OF THESE RESTRICTIONS.

COVER SHEET AND SITE PLAN

JOB NO.	1162-221
DRAWN	J.O./E.O.
CHECKED	J.C.
SCALE	NOTED
DATE	NOTED
SHEET NO.	

T100

KEYNOTE LEGEND	APPLICATIONS	BUILDING DATA	PROJECT DIRECTORY
<p>1 EXISTING ACCESSIBLE PARKING STALL WITH ACCESS AISLE, PAVEMENT MARKINGS AND SIGNAGE</p> <p>2 EXISTING ACCESSIBLE PATH OF TRAVEL. SEE PATH OF TRAVEL NOTES</p> <p>3 EXISTING UNAUTHORIZED VEHICLE PARKING IN ACCESSIBLE PARKING STALLS SIGN AT DRIVEWAY ENTRANCE</p> <p>4 EXISTING LINEAR DRAIN AT DRIVEWAY</p> <p>5 EXISTING CONCRETE STEPS</p> <p>6 EXISTING MONUMENT SIGN</p> <p>7 EXISTING FIRE HYDRANT</p> <p>8 EXISTING SCREEN WALL (FROM THIS POINT TO SOUTH END OF PROPERTY)</p> <p>9 EXISTING FIRE SPRINKLER BACKFLOW</p> <p>10 EXISTING WALKWAY</p> <p>11 EXISTING RETAINING WALL</p> <p>12 EXISTING PATH OF TRAVEL FROM PUBLIC RIGHT OF WAY</p> <p>13 EXISTING VAN ACCESSIBLE PARKING STALL WITH ACCESS AISLE, PAVEMENT MARKINGS AND SIGNAGE (LOCATED IN BASEMENT LEVEL)</p>	<p>APPLICATIONS BEING CONSIDERED ARE AS FOLLOWS:</p> <p>A. MINOR CONDITIONAL USE PERMIT (MCUP) FOR APPROVAL OF PARKING ANALYSIS UNDER THE PROVISIONS OF THE CITY OF LOMITA MUNICIPAL CODE SECTIONS 11-1.66.03 AND 11-1.66.05.</p> <p>B. VARIANCE TO ALLOW 256 S.F. OUTDOOR DINING PATIO INCLUDING ENCROACHMENT INTO 5' LANDSCAPE SETBACK.</p> <p>C. VARIANCE TO ALLOW SERVICE TYPE BUSINESSES EXCEEDING 50% CAP ON GROUND FLOOR.</p> <p>D. SITE PLAN REVIEW.</p> <p>FOLLOWING APPROVAL, OWNER WILL SUBMIT THE FOLLOWING SEPARATE PERMIT APPLICATIONS:</p> <p>1. DEMOLITION OF EXISTING NON-PERMITTED TENANT SPACE AS FOLLOWS:</p> <p>a. SUITE 207A, 207B AND 207C (ABOVE SUITE 107)</p> <p>2. BUILD OUT OF NEW VANILLA SHELL TENANT SPACE AT SUITE 108, AND 201 (MINOR ADDITION PER 11-1.66.05) INCLUDING THE FOLLOWING:</p> <p>a. INTERIOR AND EXTERIOR PARTITIONS.</p> <p>b. HVAC UNIT, DUCTING AND RESTROOM EXHAUST FAN.</p> <p>c. STOREFRONT.</p> <p>d. ELECTRICAL SUB PANEL INCLUDING CONDUIT AND FEEDERS FROM METER TO SUB PANEL.</p> <p>e. EXTERIOR SIGN CIRCUIT.</p> <p>f. ELECTRICAL CONNECTIONS AND DISCONNECT TO HVAC UNIT.</p> <p>g. EXIT SIGNS.</p> <p>h. SINGLE ACCOMMODATION ACCESSIBLE RESTROOM</p> <p>i. GYPSUM BOARD CEILING.</p> <p>j. LIGHTING AND LIGHTING CONTROLS.</p> <p>3. BUILD OUT OF NEW STORAGE ONLY SPACE, NON-LEASABLE, SUITE 208 (MINOR ADDITION PER 11-1.66.05) INCLUDING THE FOLLOWING:</p> <p>a. EXTERIOR PARTITIONS.</p> <p>b. EXHAUST FAN.</p> <p>c. ELECTRICAL SUB PANEL INCLUDING CONDUIT AND FEEDERS FROM METER TO SUB PANEL.</p> <p>d. GYPSUM BOARD CEILING.</p> <p>e. LIGHTING AND LIGHTING CONTROLS.</p> <p>4. BUILD OUT OF EXISTING TENANT SPACE FOR NEW FULL SERVICE RESTAURANT AT SUITE 101 INCLUDING THE FOLLOWING:</p> <p>a. INTERIOR PARTITIONS.</p> <p>b. HVAC UNIT, DUCTING AND RESTROOM EXHAUST FAN.</p> <p>c. STOREFRONT.</p> <p>d. ELECTRICAL SUB PANEL INCLUDING CONDUIT AND FEEDERS FROM METER TO SUB PANEL.</p> <p>e. EXTERIOR SIGN CIRCUIT.</p> <p>f. ELECTRICAL CONNECTIONS AND DISCONNECT TO HVAC UNIT.</p> <p>g. EXIT SIGNS.</p> <p>h. SINGLE ACCOMMODATION ACCESSIBLE RESTROOM.</p> <p>i. INTERIOR FINISHES.</p> <p>j. LIGHTING AND LIGHTING CONTROLS.</p> <p>k. FULL SERVICE KITCHEN INCLUDING EQUIPMENT, PLUMBING AND ELECTRICAL.</p> <p>l. OUTDOOR PATIO DINING (REQUIRES DISCRETIONARY APPROVAL)</p>	<p>APN: 7553-002-035</p> <p>SITE AREA: 33,462 S.F.</p> <p>AREA OF SITE NOT OCCUPIED BY STRUCTURES: 18,600 S.F.</p> <p>NUMBER OF STORIES IN SHELL BUILDING: TWO + BASEMENT PARKING</p> <p>BUILDING FLOOR AREA:</p> <p>EXISTING BASEMENT PARKING: 14,408 S.F.</p> <p>EXISTING FIRST FLOOR TENANT SPACE: 10,276 S.F.</p> <p>EXISTING SECOND FLOOR TENANT SPACE: 7,406 S.F.</p> <p>ADDITION FIRST FLOOR TENANT SPACE: 1,158 S.F.</p> <p>ADDITION SECOND FLOOR TENANT SPACE: 1,603 S.F.</p> <p>TYPE OF CONSTRUCTION: VB</p> <p>OCCUPANCY TYPE: B</p> <p>USE: RETAIL/OFFICE</p> <p>OCCUPANCY SEPARATIONS: NONE</p> <p>FULLY AUTOMATIC FIRE SPRINKLERS: YES</p> <p>REQUIRED LANDSCAPING (18,600 x 0.06): 1,116 S.F.</p> <p>EXISTING LANDSCAPING: 3,048 S.F.</p>	<p>PROPERTY OWNER/ APPLICANT: MAXNET COMMERCIAL 3435 OCEAN PARK BOULEVARD SUITE 107-502 SANTA MONICA, CA 90405 (213) 798-9485 CONTACT: DAVID NOSRATI</p> <p>ARCHITECT: JEFF S COFFMAN - ARCHITECT 2000 E CHAPMAN AVENUE, SUITE 202 FULLERTON, CA 92831 (714) 738-7045</p> <p>PARKING CONSULTANT: RK ENGINEERING GROUP, INC. 4000 WESTERLY PLACE, SUITE 280 NEWPORT BEACH, CA 92660 (949) 474-0809</p>



SITE PLAN

SCALE: 1"=20'-0" A

PATH OF TRAVEL, ACCESSIBLE ROUTE AND WALKING SURFACES

- ALL IMPROVEMENTS WITHIN THE PATH OF TRAVEL ARE EXISTING AS FOLLOWS:
- SLOPE IN PARKING STALL AND ACCESS AISLE SHALL NOT EXCEED 1:48 WITH 1:48 CROSS SLOPE (11B-502).
 - SLOPE OF WALKING SURFACES SHALL NOT EXCEED 1:20 WITH 1:48 CROSS SLOPE (11B-403.3).
 - THE CLEAR WIDTH OF WALKING SURFACES SHALL BE 36" MINIMUM (11B-403.5.1).
 - THE CLEAR WIDTH FOR WALKING SURFACES IN CORRIDORS SERVING AN OCCUPANT LOAD OF 10 OR MORE SHALL BE 44" MINIMUM (11B-403.5.1).
 - THE CLEAR WIDTH FOR SIDEWALKS AND WALKS SHALL BE 48" MINIMUM (11B-403.5.1).
 - THE CLEAR WIDTH FOR AISLES SHALL BE 36" MINIMUM IF SERVING ELEMENTS ON ONLY ONE SIDE, AND 44" MINIMUM IF SERVING ELEMENTS ON BOTH SIDES (11B-403.5.1).
 - WHERE THE PATH OF TRAVEL CROSSES A VEHICULAR WAY, A 36" DEEP CONTINUOUS DETECTABLE WARNING, EXTENDING THE FULL WIDTH OF THE PATH OF TRAVEL, SHALL BE LOCATED SUCH THAT THE PEDESTRIAN IS WARNED OF POTENTIAL HAZARDS, PRIOR TO ENTERING THE VEHICULAR WAY (11B-705).
 - SLOPE OF RAMPS SHALL NOT EXCEED 1:12 WITH 1:48 CROSS SLOPE. RAMP LANDINGS SHALL NOT EXCEED 1:48 SLOPE WITH 1:48 CROSS SLOPE (11B-405).
 - LANDINGS AT DOORS SHALL NOT EXCEED 1:48 WITH 1:48 CROSS SLOPE (11B-404).
 - CHANGES IN LEVEL OF 1/4" HIGH MAXIMUM SHALL BE PERMITTED TO BE VERTICAL AND WITHOUT EDGE TREATMENT (11B-303).
 - CHANGES IN LEVEL BETWEEN 1/4" HIGH MINIMUM AND 1/2" HIGH MAXIMUM SHALL BE BEVELED WITH A SLOPE NOT STEEPER THAN 1:2 (11B-303).
 - THRESHOLDS AT DOORWAYS SHALL NOT EXCEED 0.50" IN HEIGHT (11B-404.2.5)

LEGAL DESCRIPTION

PARCEL 1:
LOT(S) 109 OF TRACT NO. 848, IN THE CITY OF LOMITA, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 16 PAGE(S) 90 AND 91 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPT THEREFROM THE WESTERLY 25 FEET THEREOF CONVEYED TO THE COUNTY OF LOS ANGELES, FOR ROAD AND HIGHWAY PURPOSES, BY DEED RECORDED IN BOOK 3891, PAGE 126, OFFICIAL RECORDS.

ALSO EXCEPT THEREFROM THE NORTHERLY 10 FEET OF THE EASTERLY 112.5 FEET CONDEMNED FOR WIDENING WILMINGTON ROAD, BY FINAL DECREE OF CONDEMNATION, A CERTIFIED COPY BEING RECORDED IN BOOK 7857, PAGE 291 OFFICIAL RECORDS.

PARCEL 2:
THE EASTERLY 112.5 FEET OF LOT(S) 104 OF TRACT NO. 848, IN THE CITY OF LOMITA, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 16, PAGE(S) 90 AND 91 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN 7553-002-035

GOVERNING AUTHORITY

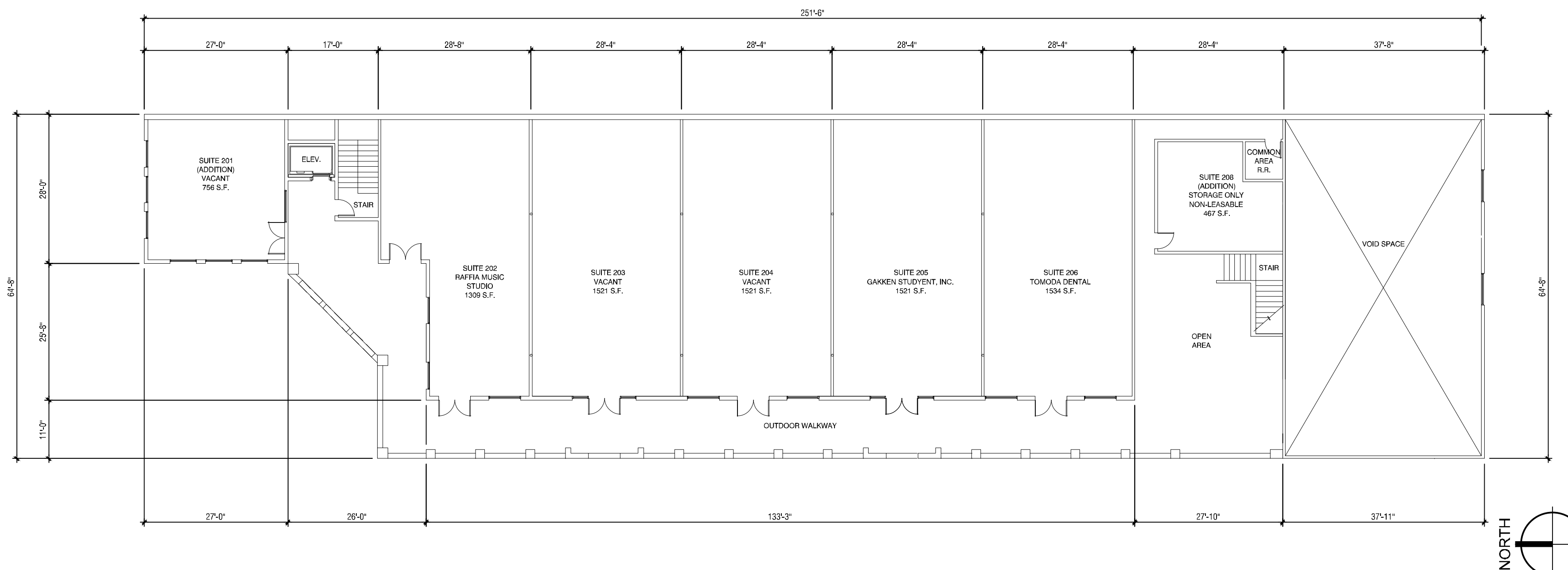
PLANNING AND BUILDING DEPARTMENT:
CITY OF LOMITA
24320 S. NARBONNE AVENUE
LOMITA, CA 90717
(310) 534-3760

APPLICABLE CODES

- CONSTRUCTION PLANS AND CALCULATIONS SHALL COMPLY FULLY WITH THE FOLLOWING:
- 2022 CALIFORNIA BUILDING CODE
 - 2022 CALIFORNIA PLUMBING CODE
 - 2022 CALIFORNIA MECHANICAL CODE
 - 2022 CALIFORNIA ELECTRICAL CODE
 - 2022 CALIFORNIA FIRE CODE
 - 2022 CALIFORNIA GREEN BUILDING STANDARDS CODE
 - 2022 CALIFORNIA ENERGY CODE / BUILDING ENERGY EFFICIENCY STANDARDS
 - CITY OF LOMITA MUNICIPAL CODE

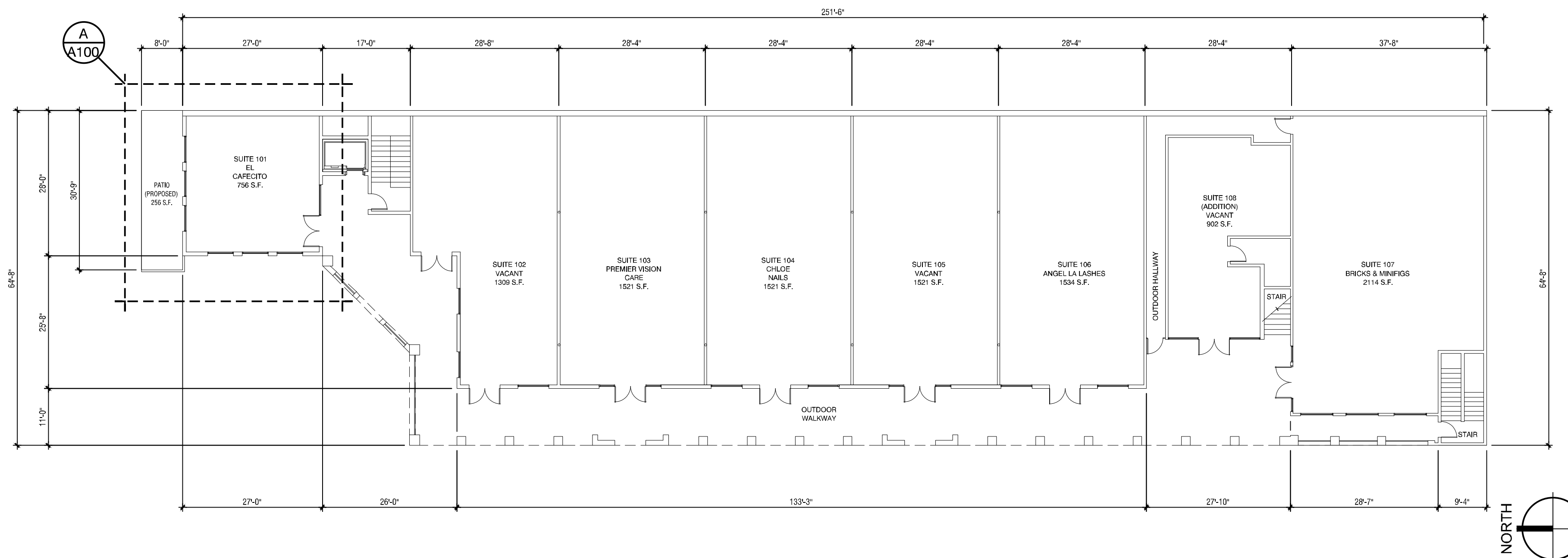
DRAWING INDEX

T100	COVER SHEET AND SITE PLAN
T200	KEY PLANS AND PARKING ANALYSIS
A100	FLOOR PLAN / PATIO PLAN - SUITE 101
A200	EXTERIOR ELEVATIONS



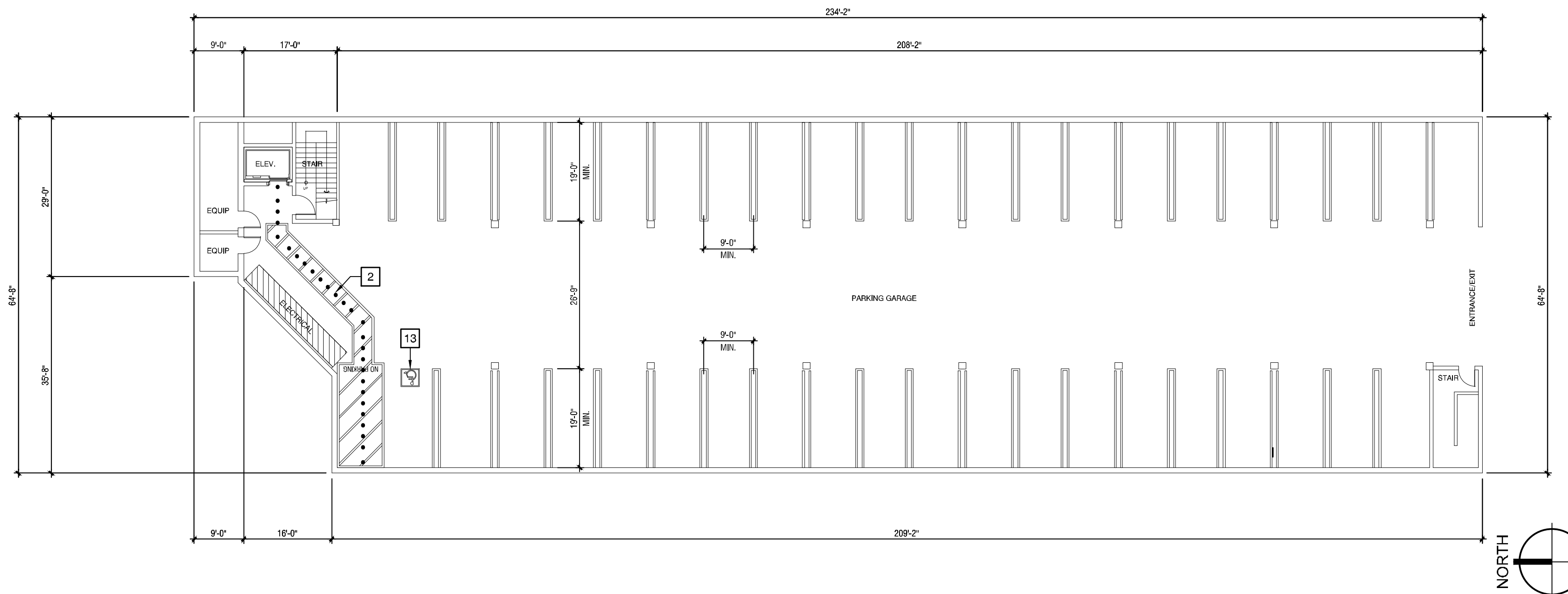
SECOND FLOOR PLAN - KEY PLAN

SCALE: 1/16" = 1'-0" A



FIRST FLOOR PLAN - KEY PLAN

SCALE: 1/16" = 1'-0" B



BASEMENT FLOOR PLAN

SCALE: 1/16" = 1'-0" C

TENANT AND PARKING ANALYSIS

FIRST FLOOR

SUITE	TENANT NAME	USE CLASS	GROSS FLR. AREA	SERVICE RELATED	% OF 1ST FLOOR	PARKING CODE ¹	PKG REQ'D
101	EL CAFECITO (PROPOSED) VACANT	RESTAURANT	756			1 PARKING SPACE PER 150 S.F. GFA (NOT LESS THAN 10 SPACES)	10
		PATIO	256	NO	9		
		SUBTOTAL - RESTAURANT	1,012				
102	VACANT	RETAIL	1,309	NO	11	1 PARKING SPACE PER 300 S.F. GFA	4.36
103	PREMIER VISION CARE	MEDICAL OFFICE	1,521	YES	13	1 PARKING SPACE PER 225 S.F. GFA	6.76
104	CHLOE NAILS	NAIL SALON	1,521	YES	13	1 PARKING SPACE PER 300 S.F. GFA	5.07
105	VACANT	RETAIL	1,521	NO	13	1 PARKING SPACE PER 300 S.F. GFA	5.07
106	ANGEL LA LASHES	BEAUTY SALON	1,534	YES	13	1 PARKING SPACE PER 300 S.F. GFA	5.11
107	BRICKS & MINIFIGS	RETAIL	2,114	NO	19	1 PARKING SPACE PER 300 S.F. GFA	7.05
108	VACANT (ADDITION)	RETAIL	902	NO	8	1 PARKING SPACE PER 300 S.F. GFA	3.01
RETAIL/RESTAURANT USE - FIRST FLOOR:			5,700				
SERVICE RELATED USE - FIRST FLOOR:			4,576		40		
ADDED FLOOR AREA - LEASABLE (11-1.66.05): ² (SUITE 108 + PATIO)			1,158				
GROSS LEASABLE FLOOR AREA - FIRST FLOOR:			11,434				
REQUIRED PARKING - FIRST FLOOR:							46.43

SECOND FLOOR

SUITE	TENANT NAME	USE CLASS	GROSS FLR. AREA	SERVICE RELATED	% OF 2ND FLOOR	PARKING CODE ¹	PKG REQ'D
201	VACANT (ADDITION)	RETAIL	756	NO	9	1 PARKING SPACE PER 300 S.F. GFA	2.52
202	RAFFIA MUSIC STUDIO	MUSIC LESSONS	1,309	YES	15	1 PARKING SPACE PER 300 S.F. GFA	4.36
203	VACANT	RETAIL	1,521	NO	18	1 PARKING SPACE PER 300 S.F. GFA	5.07
204	VACANT	RETAIL	1,521	NO	18	1 PARKING SPACE PER 300 S.F. GFA	5.07
205	GAKKEN STUDYENT, INC.	TUTORING	1,521	YES	18	1 PARKING SPACE PER 300 S.F. GFA	5.07
206	TOMODA DENTISTRY	MEDICAL OFFICE	1,534	YES	18	1 PARKING SPACE PER 225 S.F. GFA	6.82
207A	SPACE DEMOLISHED						
207B	SPACE DEMOLISHED						
207C	SPACE DEMOLISHED						
208	STORAGE (ADDITION)	NON-LEASABLE	467	NO	5		
209	SPACE DEMOLISHED						
RETAIL/OFFICE/SERVICE RELATED USE - SECOND FLOOR:			7,406				
ADDED FLOOR AREA - LEASABLE (11-1.66.05): ²			756				
ADDED FLOOR AREA - NON LEASABLE			467				
GROSS LEASABLE FLOOR AREA - SECOND FLOOR:			8,162				
REQUIRED PARKING - SECOND FLOOR:							28.91

OVERALL TOTALS:							
GROSS LEASABLE FLOOR AREA - OVERALL (EXCL. ADDED FLOOR AREA):			17,682				
ADDED FLOOR AREA - LEASABLE - OVERALL:			1,914				
ADDED FLOOR AREA - NON LEASABLE - OVERALL:			467				
TOTAL GROSS LEASABLE FLOOR AREA:			19,596				
TOTAL PARKING SPACES REQUIRED PER CODE (46.43 + 28.91): ¹							76
TOTAL PARKING SPACES PROVIDED:							71
10% INTENSIFICATION CREDIT: ²							7
PARKING ALLOWANCE WITH CREDIT:							78

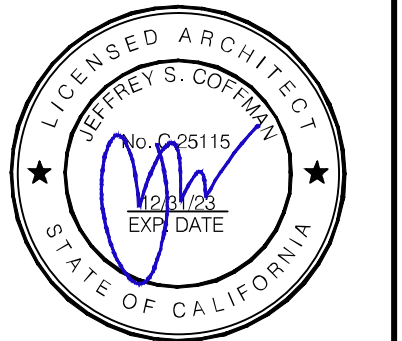
COMMENTS	
10/27/22	- ISSUED FOR PLANNING REVIEW
11/17/22	- ISSUED FOR PLANNING REVIEW
12/07/22	- ISSUED FOR PLANNING REVIEW
02/24/23	- ISSUED FOR PLANNING REVIEW
04/10/23	- SUBMIT PLANNING APPLICATIONS
05/23/23	- PLANNING COMMENTS

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¹ THE PARKING REQUIREMENTS ARE BASED ON THE CITY OF LOMITA MUNICIPAL CODE SECTION 11-1.66.03 PARKING REQUIREMENTS
² SECTION 11-1.66.05: OFF-STREET PARKING FACILITIES SHALL BE PROVIDED FOR ANY NEW BUILDING CONSTRUCTED AND ANY ADDITION OR EXPANSION OF AN EXISTING BUILDING IN ORDER TO MEET THE DEMAND OF ALL ACTIVITIES ON THE LOT. ADDITIONAL OFF-STREET PARKING FACILITIES SHALL NOT BE REQUIRED FOR MINOR ADDITIONS TO BUILDINGS LOCATED IN COMMERCIAL AND MANUFACTURING ZONES. A MINOR ADDITION OR EXPANSION SHALL MEAN A CHANGE IN USE, EXPANSION OF USE, OR BUILDING ADDITION WHICH WOULD INCREASE THE REQUIRED NUMBER OF PARKING SPACES BY TEN (10) PERCENT OR LESS.

JEFF S. COFFMAN ARCHITECT
 architecture
 planning
 research

2000 e chapman avenue
 suite 202
 fullerton, ca 92831
 714.738.7045 p:



APPLICATION FOR APPROVAL OF PARKING ANALYSIS TO SERVE EXISTING TWO STORY RETAIL/OFFICE BUILDING

FOR:
 PENINSULA PLAZA

25820 LUCILLE AVE
 LOMITA, CA 90717

REVISIONS

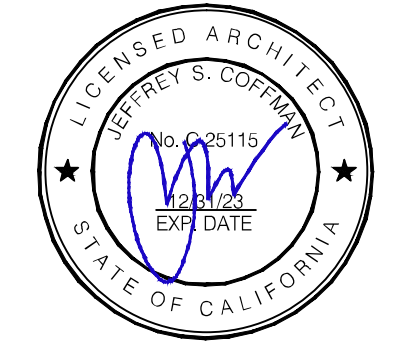
COMMENTS

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KEY PLANS AND PARKING ANALYSIS

JOB NO.	1162-221
DRAWN	J.O./E.O.
CHECKED	J.C.
SCALE	NOTED
DATE	NOTED
SHEET NO.	

T200



APPLICATION FOR
APPROVAL OF
PARKING ANALYSIS
TO SERVE EXISTING
TWO STORY
RETAIL/OFFICE
BUILDING

FOR:
PENINSULA PLAZA

25820 LUCILLE AVE
LOMITA, CA 90717

REVISIONS

NO.	DATE	DESCRIPTION

COMMENTS

10/27/22	- ISSUED FOR PLANNING REVIEW
11/17/22	- ISSUED FOR PLANNING REVIEW
12/07/22	- ISSUED FOR PLANNING REVIEW
02/24/23	- ISSUED FOR PLANNING REVIEW

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FLOOR PLAN

JOB NO.	1162-221
DRAWN	J.O./E.O.
CHECKED	J.C.
SCALE	NOTED
DATE	NOTED

SHEET NO.

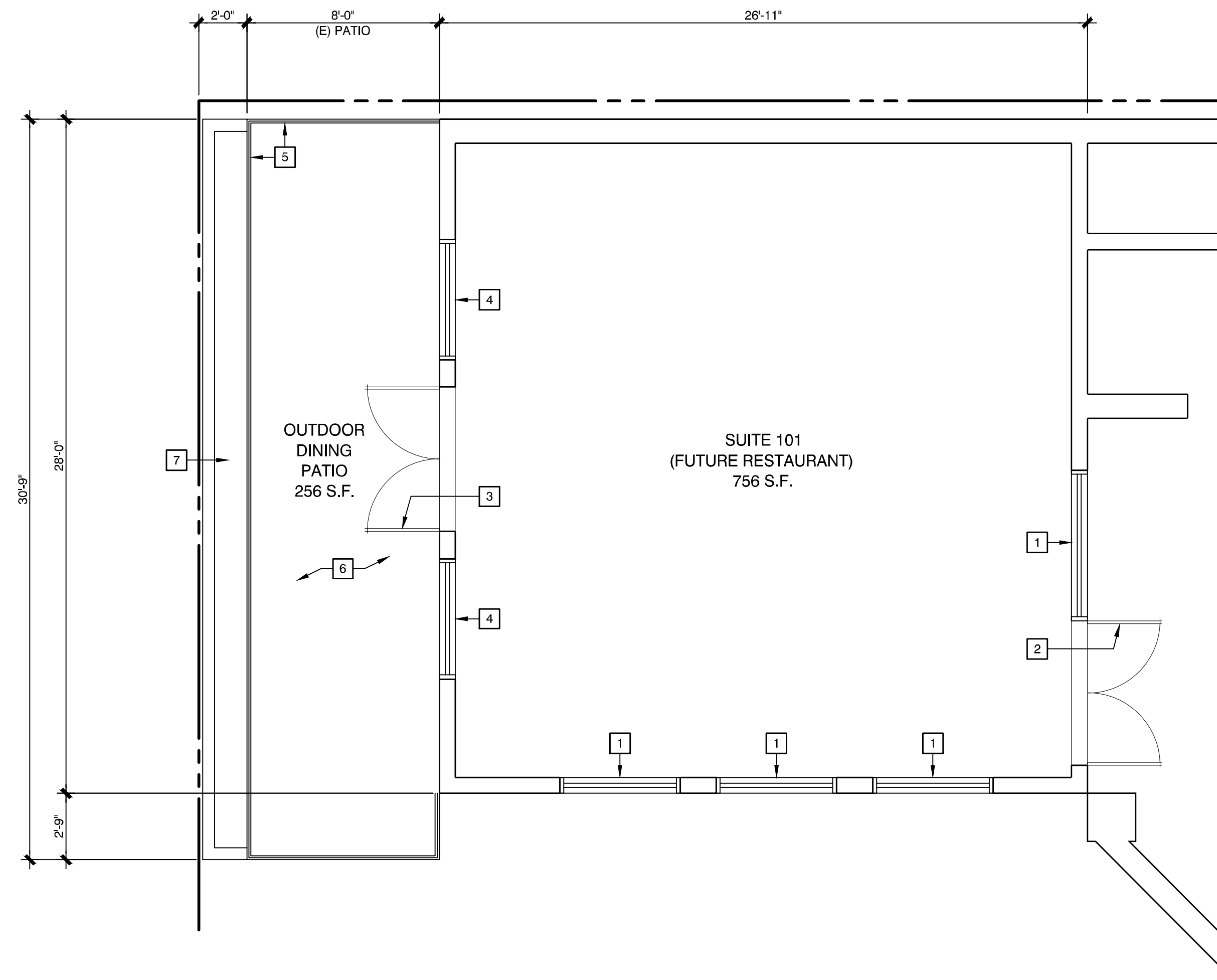
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KEYNOTE LEGEND

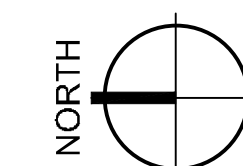
- 1 EXISTING STOREFRONT
- 2 EXISTING STOREFRONT DOORS
- 3 REPLACE EXISTING STOREFRONT WITH PAIR OF 3'-0" WIDE STOREFRONT DOORS TO MATCH EXISTING
- 4 REPLACE EXISTING ARCHED STOREFRONT TO MATCH RECTANGULAR STYLE OF NEW DOORS
- 5 42" HIGH TEMPERED GLASS GUARDRAIL
- 6 WOOD PLANK PATIO DECK
- 7 EXISTING PLANTER

SYMBOL LEGEND

- EXISTING WALL
- ▬ EXISTING STOREFRONT

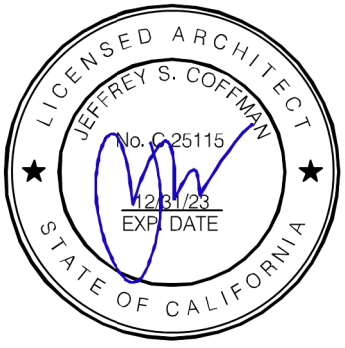


FLOOR PLAN



SCALE:
1/4" = 1'-0"

A



APPLICATION FOR
APPROVAL OF
PARKING ANALYSIS
TO SERVE EXISTING
TWO STORY
RETAIL/OFFICE
BUILDING

FOR:
PENINSULA PLAZA

25820 LUCILLE AVE
LOMITA, CA 90717

REVISIONS

COMMENTS

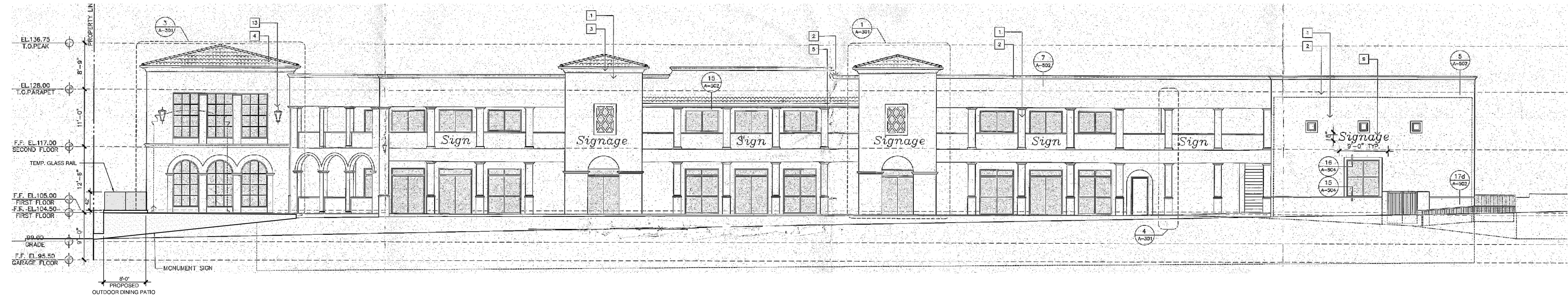
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REVIEW
11/17/22 - ISSUED FOR PLANNING
REVIEW
12/07/22 - ISSUED FOR PLANNING
REVIEW
02/24/23 - ISSUED FOR PLANNING
REVIEW

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THESE RESTRICTIONS.

EXISTING
EXTERIOR
ELEVATIONS

JOB NO. 1162-221
DRAWN J.O./E.O.
CHECKED J.C.
SCALE NOTED
DATE NOTED
SHEET NO.

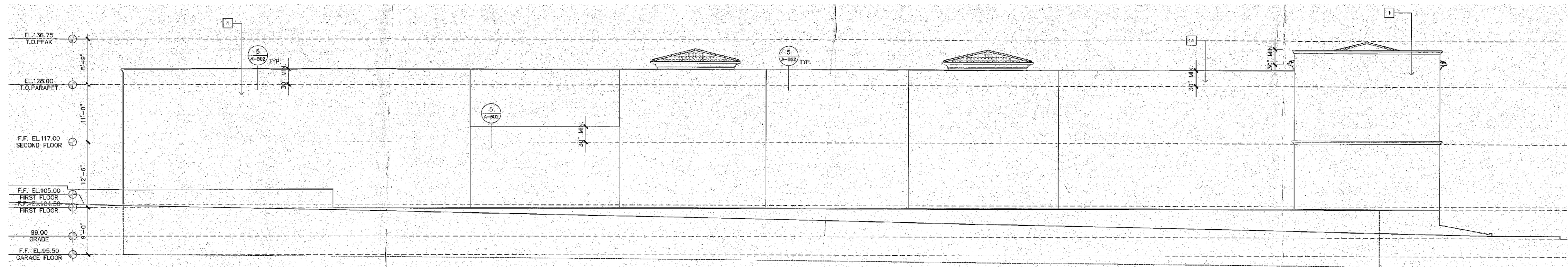
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EXISTING WEST ELEVATION WITH PROPOSED PATIO

SCALE:
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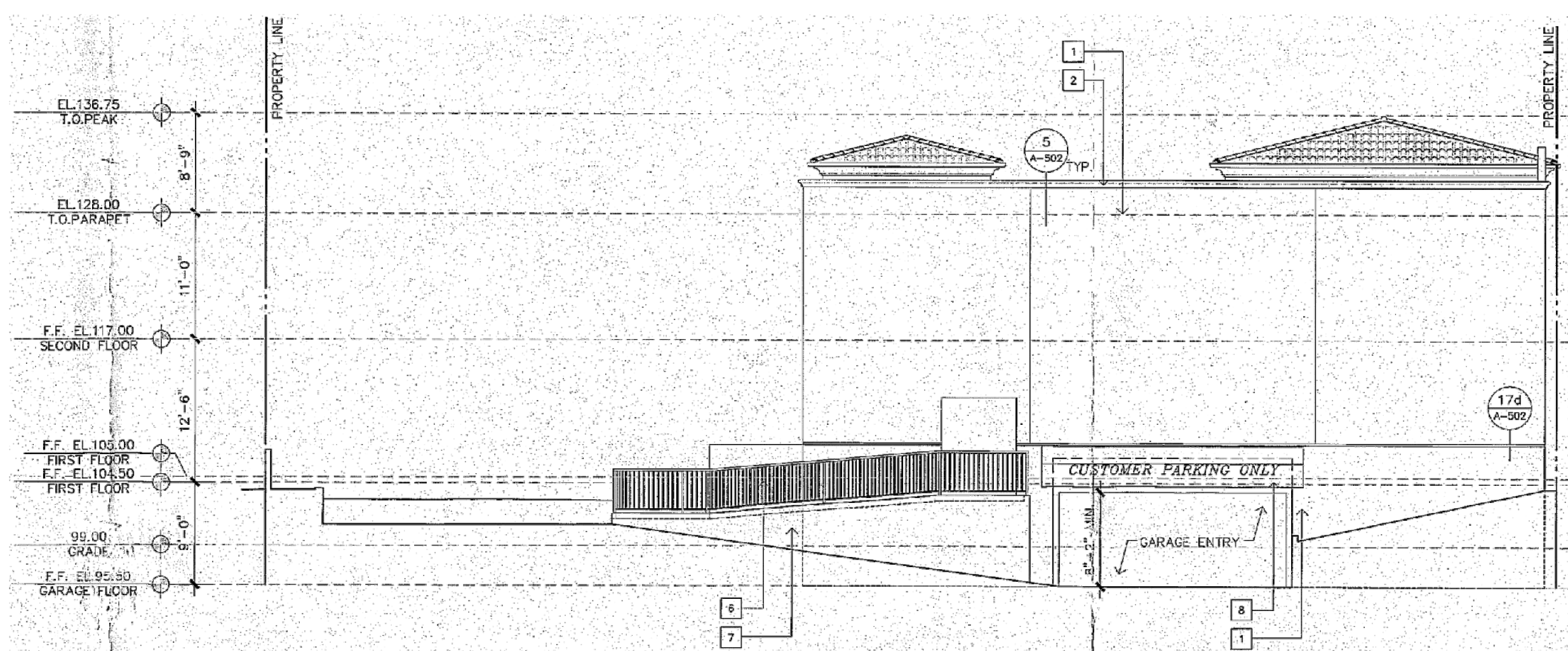
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EXISTING EAST ELEVATION

SCALE:
3/32"=1'-0"

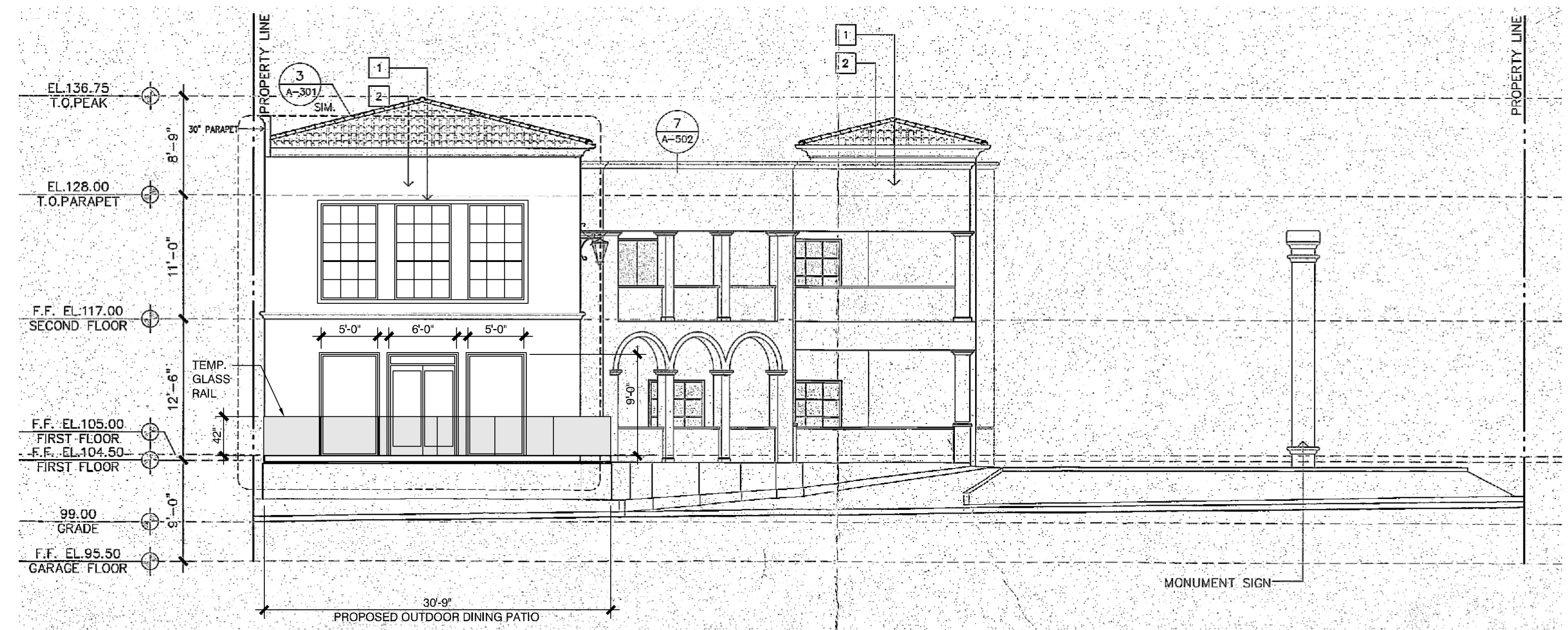
B



EXISTING SOUTH ELEVATION

SCALE:
3/32"=1'-0"

D



EXISTING NORTH ELEVATION WITH PROPOSED PATIO

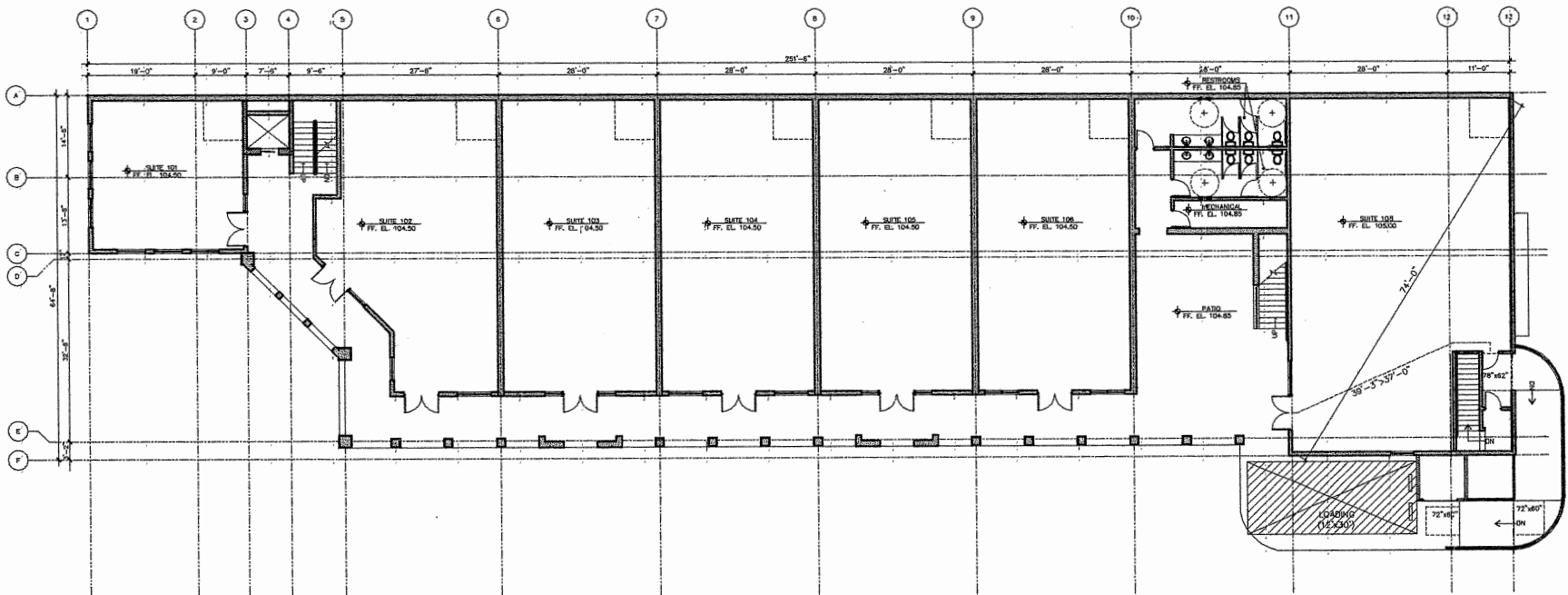
SCALE:
1/8"=1'-0"

C

The Board, agents and those who have been authorized to act for the Board, shall not be liable for any errors or omissions in this plan or for any consequences thereof.



ASHAI DESIGN
 3035 Kashwa Street
 Suite # 102
 Torrance, CA 90505
 Tel: (310) 534-9382
 Fax: (310) 534-9390
 Tony Ashol, AIA
 Arthur Ashol, AIA



1 FIRST FLOOR PLAN
 SCALE: 1/8" = 1'-0"

REVISIONS	
No.	Description

ISSUED
 Issued for:
 PLAN CHECK
 SECOND
 CONSTRUCTION

PROJECT TITLE
 NEW RETAIL
 COMMERCIAL
 BUILDING

JOB ADDRESS
 2244 PACIFIC COAST HWY.
 LOMITA CA, 90371

OWNER
 3043 KASHWA LLC

LEGAL DESCRIPTION
 LOT 100 AND A PORTION
 OF LOT 104, TRACT: 968

DATE
 08/13/07
 DRAWN BY
 T.A.
 CHECKED BY
 T.A.

Drawing Type
 Drawing No.
A-103
 SHEET NO. OF SHEETS

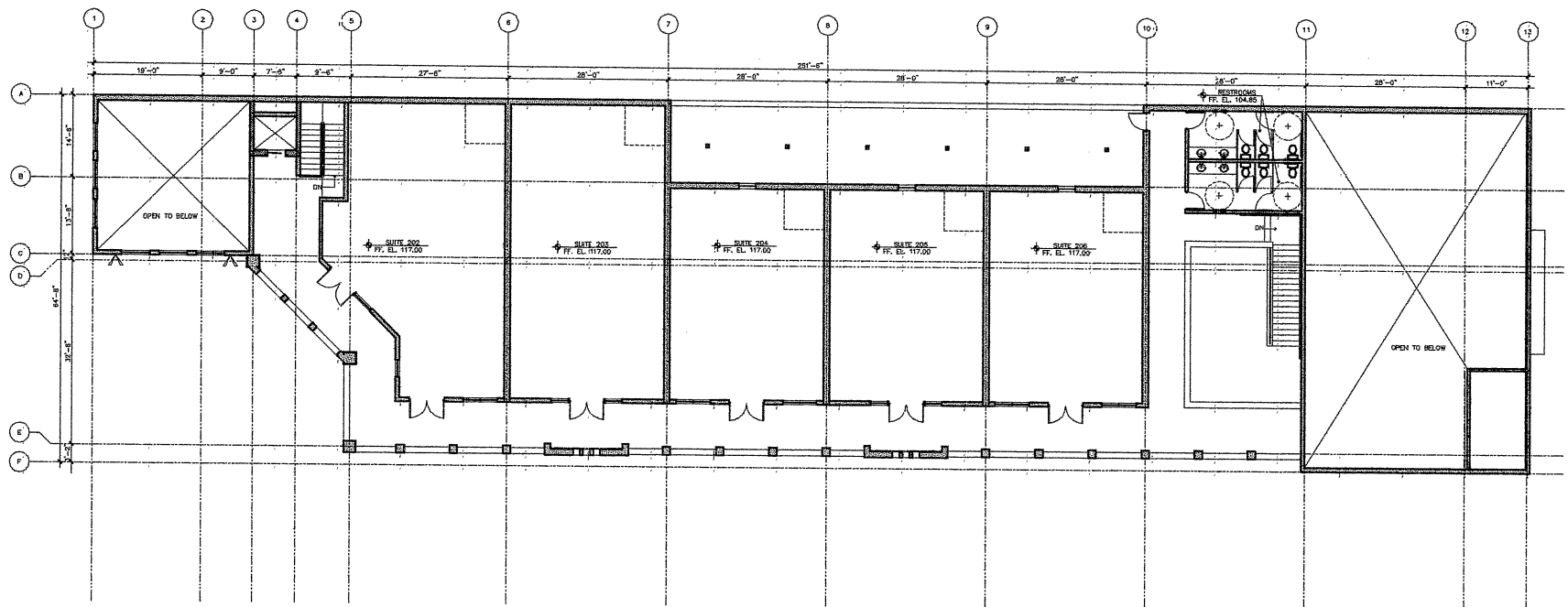
The owner, architect and those authorized to act for them shall be responsible for the accuracy of the information provided and for the accuracy of the drawings and specifications. The architect shall not be responsible for the accuracy of the information provided by the owner or for the accuracy of the drawings and specifications prepared by the owner or for the accuracy of the information provided by the owner or for the accuracy of the drawings and specifications prepared by the owner.



ASHAI DESIGN
 3035 Kashwa Street
 Suite # 102
 Torrance, CA 90505
 Tel: (310) 534-9382
 Fax: (310) 534-8390

Tony Ashol, AIA
 Arthur Ashol, AIA

ASAI



REVISIONS		
No.	Date	Remarks

ISSUED
 FOUND RE
 PLAN CHECK
 RECORD
 CONSTRUCTION

PROJECT TITLE
**NEW RETAIL
 COMMERCIAL
 BUILDING**

JOB ADDRESS
 2244 PACIFIC COAST HWY.
 LOMITA CA, 90717

OWNER
 3035 KASHWA LLC

LEGAL DESCRIPTION
 LOT 109 AND A PORTION
 OF LOT 104, TRACT: 848

SHEET NO
**SECOND
 FLOOR PLAN**

1 SECOND FLOOR PLAN
 SCALE: 1/8" = 1'-0"

JOB NO: J-0407
 DATE: 09/12/07
 DRAWN BY: EJM
 CHECKED BY: T.A.
 PLOTTED BY:

Drawing Type: Drawing No:

A-104

SHEET NO. OF SHEETS

April 4, 2023

Mr. David Nosrati
MAXNET COMMERCIAL
3435 Ocean Park Boulevard
Suite 107-502
Santa Monica, CA 90405

Subject: Peninsula Plaza ULI Shared Parking Analysis, City of Lomita

Dear Mr. Nosrati:

Introduction

RK ENGINEERING GROUP, INC. (RK) is pleased to provide this shared parking study for the two-story mixed-use building located at 25820 Lucille Avenue, in the City of Lomita. Peninsula Plaza is an existing mixed-use development consisting of restaurant, retail, medical office, hair salon, tutoring, uses etc. The site consists of approximately 20,063 gross square feet of rentable tenant space. A total of seventy-one (71) parking spaces are provided in a surface parking lot and are exclusively shared by all tenants.

The location map for the project is included in Exhibit A and the site plan for the project is shown in Exhibit B.

The City of Lomita is requesting a parking study to determine the adequacy of parking for the project site. The City has provided the following scope of work which will be addressed in this parking study:

1. The peak hour parking demand from all uses does not coincide and/or the uses are such that the hours of operation are different for various portions of the businesses;
2. The quantity circulation and location of parking provided will equal or exceed the level that can be expected if to share or off-site parking is not provided;

3. The adjacent properties will not be adversely affected relative to parking;
4. The proposed traffic circulation will not be detrimental to the health, safety and welfare of residents residing or working nearby; and
5. Off-site parking is within 300 ft. legal distance or suitable distance.

To determine the adequacy of parking for the project, RK will calculate the parking requirements for the project based on the City of Lomita Municipal Code (Section 11-1.66.03) Parking Requirements. Furthermore, RK will utilize the Urban Land Institute (ULI) Shared Parking Methodology, Third Edition with the City of Lomita parking requirements to determine the peak monthly, hourly weekday and be parking demands for the mixed-use project with a 100% tenant occupancy scenario for all other uses within the mixed-use building.

The shared parking analysis and corresponding conclusions presented in this parking needs analysis are based on recommendations and data presented by the Urban Land Institute (ULI), the International Council of Shopping Centers (ICSC), National Parking Association, and specifically the ULI shared parking methodology, using ULI-supplied data as presented in the current ULI Shared Parking Model, Third Edition, published in 2020.

City of Lomita Municipal Code (Section 11-1.66.03) Parking Requirements

Table 1 identifies the parking requirements per land use within the project based on the City of Lomita Municipal Code. Based on the results of the parking code calculations, the project requires a total of 75 parking spaces. Based on Sec. 11-1.66.05: (A) Off-street parking facilities shall be provided for any new building constructed and any addition or expansion of an existing building to meet the demand of all activities on the lot. Additional off-street parking facilities shall not be required for minor additions to buildings located in commercial and manufacturing zones. A minor addition or expansion shall mean a change in use, expansion of use, or building addition which would increase the required number of parking spaces by ten (10) percent or less. Considering the 10% parking credit, the project is consistent with the parking requirements as identified in Table 1.

The City's parking code requirements do not consider the sharing of uses within a mixed-use site, and since the project has a mix of land uses within a shared site, the number of parking spaces required by code are overstated because all uses are calculated to be parked individually and not shared.

The project consists of compatible land uses that lend themselves well to shared parking. For example, the general office use tenants typically have a peak parking demand during the daytime while the restaurant will peak in the evening hours. As a result of the non-conflicting peak parking demand times, the uses will generate a lower parking demand.

In addition, the project location is in proximity to public transit, ride-hailing, pedestrian access, bicycle access, etc. As a result, the parking demand for the project is reduced because not all visitors will self-park onsite.

This parking needs analysis utilizes the ULI Shared Parking methodology to help ensure that the project's parking demand can be accommodated during the busiest hours, days, and months out of the year.

City of Lomita Municipal Code Chapter 25.52 is provided in Appendix B.

ULI Shared Parking Analysis

Shared parking methodology was first developed in the 1980's and has been a widely accepted industry standard for determining parking demands for mixed use projects. Adopted by cities throughout the U.S., and codified in zoning ordinances as an acceptable practice, shared parking is endorsed by the ULI, the American Planning Association (APA), the National Parking Association (NPA), and International Council of Shopping Centers (ICSC), as an acceptable method of parking planning and management.

RK used the procedures developed by the ULI from its 2020 publication, *Shared Parking*, Third Edition. This document contains the latest procedures and data with respect to parking demand and shared parking. The parking demand rates for individual land uses were derived from the City of Lomita Municipal Parking Code.

Shared parking allows for the sharing of parking spaces among uses in a mixed-use environment—in lieu of providing a minimum number of parking spaces for each individual use. Shared parking commonly results in a reduction of required parking spaces. This reduction, which is sometimes significant, depends on the quantities, mix of uses, site operational characteristics, and local code requirements.

The ULI shared parking analysis evaluates the types of land uses, parking rates, monthly variations of parking demand by land use, differences between weekday and weekend parking demand, the hourly distribution of peak parking demand for each type of land use, and captive versus non-captive parking demand within the project site. The ULI procedures were utilized within this assessment to evaluate the peak parking demand that will occur at the Landmark Plaza mixed-use shopping center.

The following inputs were included within the shared parking computer program for each land use:

1. Peak parking demand by land use for visitors and employees.
2. Captive versus non-captive parking demand.
3. Driving adjustments for alternative modes of transportation.
4. Hourly Variations of parking demand.
5. Weekday versus weekend adjustment factor.
6. Monthly adjustment factors to account for variations in parking demand over the year.
7. City of Lomita Municipal Parking Code for parking rates. (As a conservative measure, the 10% parking credit is not being applied to the ULI calculations.)

The peak parking demand was determined by utilizing the City of Lomita's Municipal Parking Code, parking data contained within the ULI Shared Parking Model, (Third Edition), evaluating peak demand during weekdays and weekends for all months throughout a year, and the operational characteristic of the proposed land uses within the site.

Appendix A includes the ULI Shared Parking Manual, Third Edition

ULI Shared Parking Calculations

As described in the above section, the ULI Shared Parking Model was used to evaluate the peak parking demand for the project. The model provides the following parking demand data: monthly variations for each land use, different peaking characteristics between weekday and weekend, hourly distribution of peak parking demand for each land use, modal adjustments, and captive versus non-captive parking demand. Furthermore, RK incorporated the City of Lomita parking rates within the ULI model.

The ULI Shared Parking Model also proportions the parking rates between visitors and employees for weekday and weekend conditions, each with their own parking demand characteristics. While the ULI parking rates were modified to reflect the City of Lomita Parking Code, the split between employees and visitors identified in the ULI analysis was used.

ULI Shared Parking Results (Tables 2,3 and Graphs 4 and 5)

The objective of this analysis is to provide a reasonable recommendation for the adequate number of on-site parking spaces needed during all times of the year. The results consider the site in operation with a full 100% tenant occupancy scenario. The findings of the shared parking model are as follows:

1. The weekday estimated peak-hour parking demand is projected to occur in December at 1:00 PM, as indicated in Table 2. The weekday peak parking demand requires a total of 67 parking spaces. Since the site has a total parking allocation of 71 parking spaces, there are 4 parking spaces available during the peak weekday parking demand time.

2. The weekend estimated peak-hour parking demand is projected to occur in December at 1:00 PM, as indicated in Table 2. The weekend peak parking demand requires a total of 60 parking spaces. Since the site has a total parking allocation of 71 parking spaces, there are 11 parking spaces available during the peak weekday parking demand time.
3. Table 3 shows a summary of the projected monthly weekday and weekend peak parking demand for the project for the year. As shown in Table 3, the peak parking demand occurs during December. Based on the parking demand data, the project has enough parking available at all times of the year.
4. Graph 1 provides the Weekday Peak Month Daily Parking Demand by Hour for all tenants onsite.
5. Graph 2 provides the Weekend Peak Month Daily Parking Demand by Hour for all tenants onsite.

Summary of Findings

RK has completed the ULI Shared Parking analysis. Based upon this study, RK has determined the peak parking demand and developed a shared parking assessment.

1. The project consists of a mixed-use two-story building consisting of retail, restaurant, hair salon, medical office, and restaurant uses, which are compatible from a shared parking standpoint. Peak parking demand will not occur simultaneously for all uses.
2. The site has a total parking allocation of 71 parking spaces and is shared by all tenants.

3. The parking calculations utilize the City of Lomita's Municipal Code Parking Requirements.

4. Utilizing the shared parking concept, the peak parking demand for the project has been estimated to be 67 parking spaces during peak weekday conditions at 1:00 PM in December and 60 parking spaces during peak weekend conditions at 1:00 PM in December. Since the project has a parking supply of 71 spaces, there are enough parking spaces provided to support the peak parking demands.

Response to City Scope of Work:

The City of Lomita has requested that the parking study addresses the following items:

***Comment #1:** The peak hour parking demand from all uses does not coincide and/or the uses are such that the hours of operation are different for various portions of the businesses.*

RK Response: The site consists of a complimentary land use mix that has nonconflicting peak parking demand times. Based on the results of the ULI Shared Parking Analysis, it is evident that the peak parking demands of the project do not create a parking deficiency onsite when assuming a 100% tenant occupancy scenario.

Comment #2: The quantity circulation and location of parking provided will equal or exceed the level that can be expected if to share or off-site parking is not provided.

RK Response: Based on the results of the ULI Shared Parking Analysis, the 71 parking spaces provided onsite can accommodate the parking demands of the project without creating an overflow parking situation.

Comment #3: *The adjacent properties will not be adversely affected relative to parking.*

RK Response: The results of the ULI Shared Parking Analysis indicate that the weekday peak parking demand is 67 parking spaces, and the weekend peak parking demand is 60 parking spaces. Since the project provides a total of 71 parking spaces, there are enough parking spaces provided onsite. As a result, adjacent properties will not be adversely affected.

Comment #4: *The proposed traffic circulation will not be detrimental to the health, safety and welfare of residents residing or working nearby; and*

RK Response: The results of the ULI Shared Parking Study indicate that the 71 parking spaces provided on site are sufficient to accommodate the peak parking demands of the project. As a result, the proposed traffic circulation will not be detrimental to the health, safety and welfare of residents residing or working nearby.

Comment #5: *Off-site parking is within 300 ft. legal distance or suitable distance.*

RK Response: The project does not need to rely on off-site parking. As indicated by the results of the parking analysis, the 71 parking spaces provided can accommodate the weekday and weekend peak parking demands of the project.

Conclusion:

RK has completed the ULI shared parking analysis for the mixed-use shopping center. Based on the results of the analysis, the project has enough parking spaces to support the parking demand during all times of the year assuming a 100% full tenant occupancy scenario.

If you have any questions regarding this study, or need further review, please do not hesitate to call our office at (949) 474-0809.

Kind regards,

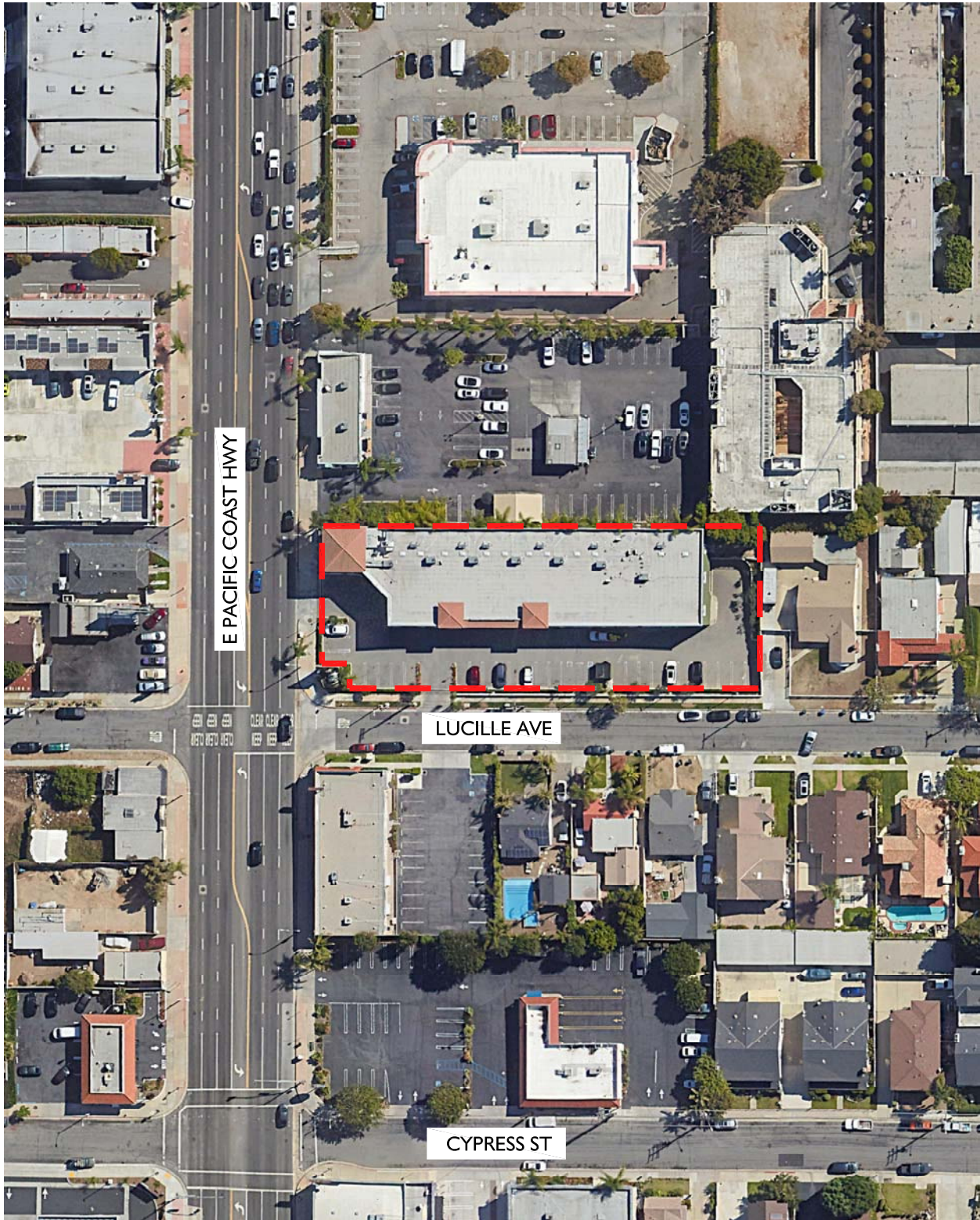
A handwritten signature in black ink that reads "Rogier Goedecke". The signature is written in a cursive style with a large initial 'R'.

Rogier Goedecke
President

RK18060

JN:3060-2022-01

Exhibits



Legend:

--- = Project Site

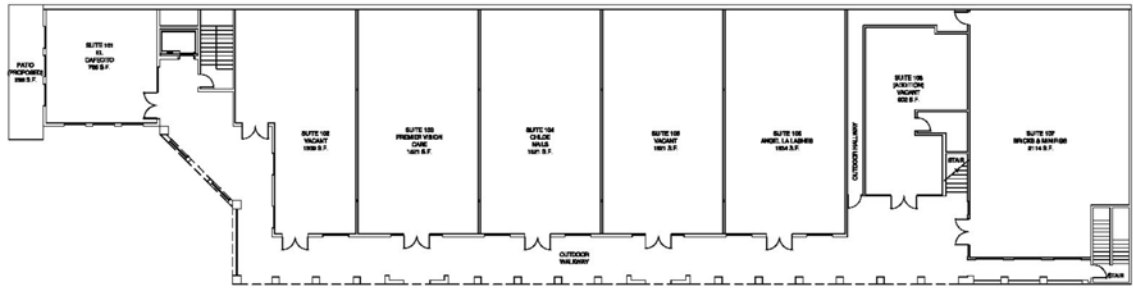




SECOND FLOOR PLAN - KEY PLAN



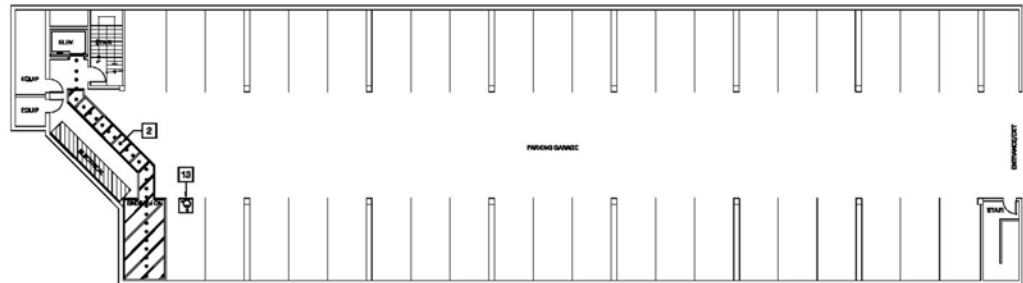
A



FIRST FLOOR PLAN - KEY PLAN



B



BASEMENT FLOOR PLAN



C



Tables & Graphs

**Table 1
Peninsula Plaza
25820 Lucille Avenue Mixed-Use Building
City of Lomita Required Parking (Sec 11-1.66.03)¹**

Suite	Tenant	Land Use	Gross Floor Area	% of Gross Floor Area	Parking Code	Required Parking	
FIRST FLOOR							
101	El Cafecito (Proposed) VACANT	RESTAURANT	756	9%	1 parking space per 150 SF GFA (Not less than 10 spaces)	10	
		Patio	256				
		Subtotal Restaurant	1,012				
102	VACANT	RETAIL	1,309	11%	1 parking space per 300 SF GFA	4	
103	Premier Vision Care	MEDICAL OFFICE	1,521	13%	1 parking space per 225 SF GFA	7	
104	Chloe Nails	NAIL SALON	1,521	13%	1 parking space per 300 SF GFA	5	
105	VACANT	RETAIL	1,521	13%	1 parking space per 300 SF GFA	5	
106	Angel La Lashes	BEAUTY SALON	1,534	13%	1 parking space per 300 SF GFA	5	
107	Bricks & Minifigs	RETAIL	2,114	18%	1 parking space per 300 SF GFA	7	
108	VACANT	RETAIL	902	8%	1 parking space per 300 SF GFA	3	
			Subtotal First Floor	11,434	100%	Subtotal First Floor Required Parking	46
SECOND FLOOR							
201	VACANT	RETAIL	756	9%	1 parking space per 300 SF GFA	3	
202	Raffia Music Studio	MUSIC LESSONS	1,309	15%	1 parking space per 300 SF GFA	4	
203	VACANT	RETAIL	1,521	18%	1 parking space per 300 SF GFA	5	
204	VACANT	RETAIL	1,521	18%	1 parking space per 300 SF GFA	5	
205	Gakken Student, Inc.	TUTORING	1,521	18%	1 parking space per 300 SF GFA	5	
206	Tomoda Dentistry	MEDICAL OFFICE	1,534	18%	1 parking space per 225 SF GFA	7	
207A	SPACE DEMOLISHED						
207B	SPACE DEMOLISHED						
207C	SPACE DEMOLISHED						
208	STORAGE	NON-LEASABLE	467	5%			
209	SPACE DEMOLISHED						
			Subtotal Second Floor	8,629	100%	Subtotal Second Floor Required Parking	29
			Total Building Gross Square Feet	20,063	100%	Total Parking Spaces Required per Code	75
						Total Parking Spaces Provided	71
						Total Parking Spaces Provided w/10% Intensification Credit²	78
						Number of Parking Spaces in Surplus per Code	3
						Percent of Parking Spaces in Surplus per Code	4%

¹ The parking requirements are based on the City of Lomita Municipal Code Section 11-1.66.03 Parking Requirements

² Sec. 11-1.66.05: (A) Off-street parking facilities shall be provided for any new building constructed and any addition or expansion of an existing building in order to meet the demand of all activities on the lot. Additional off-street parking facilities shall not be required for minor additions to buildings located in commercial and manufacturing zones. A minor addition or expansion shall mean a change in use, expansion of use, or building addition which would increase the required number of parking spaces by ten (10) percent or less.

**Table 2
Peninsula Plaza
25820 Lucille Avenue Mixed-Use Building
Shared Parking Demand Summary Table¹**

Shared Parking Demand Summary																		
Peak Month: DECEMBER -- Peak Period: 1 PM, WEEKDAY																		
Land Use	Project Data		Weekday					Weekend					Weekday			Weekend		
			Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Base Ratio	Driving Adj	Non-Captive Ratio	Project Ratio	Unit For Ratio	Peak Hr Adj	Peak Mo Adj	Estimated Parking Demand	Peak Hr Adj	Peak Mo Adj	Estimated Parking Demand
	Quantity	Unit										1 PM	December		1 PM	December		
Retail																		
Retail (<400 ksf) Employee	10,111	sf GLA	2.66	95%	100%	2.53	ksf GLA	2.66	95%	100%	2.53	ksf GLA	100%	100%	26	100%	100%	26
			0.67	90%	100%	0.60		0.67	90%	100%	0.61		100%	100%	6	100%	100%	6
Food and Beverage																		
Fast Casual/Fast Food Employee	1,012	sf GLA	8.60	95%	75%	6.13	ksf GLA	8.60	95%	75%	6.13	ksf GLA	100%	100%	6	100%	100%	6
			1.40	90%	100%	1.26		1.40	90%	100%	1.26		100%	100%	2	100%	100%	2
Medical Office																		
Medical/Dental Office Employee	3,055	sf GFA	2.89	100%	90%	2.60	ksf GFA	0.00	100%	90%	0.00	ksf GFA	90%	100%	7	0%	100%	-
			1.55	90%	100%	1.40		0.00	90%	100%	0.00		100%	100%	5	0%	100%	-
Salon & Tutoring																		
Salon's Employee	3,055	sf GFA	2.66	95%	90%	2.27	sf GFA	2.66	95%	90%	2.28	sf GFA	75%	100%	6	100%	100%	8
			0.67	90%	100%	0.60		0.67	90%	100%	0.61		90%	100%	2	100%	100%	3
Music/Tutoring Lessons Employee	2,830	sf GFA	2.66	95%	90%	2.27	sf GFA	2.66	95%	90%	2.28	sf GFA	75%	100%	5	100%	100%	7
			0.67	90%	100%	0.60		0.67	90%	100%	0.61		90%	100%	2	100%	100%	2
															50		Customer/Visitor	47
															17		Employee/Resident	13
															67		Total Demand	60
															71		Total Parking Spaces Provided On-Site	71
															4		Total Parking Spaces Available at Peak Demand Time	11

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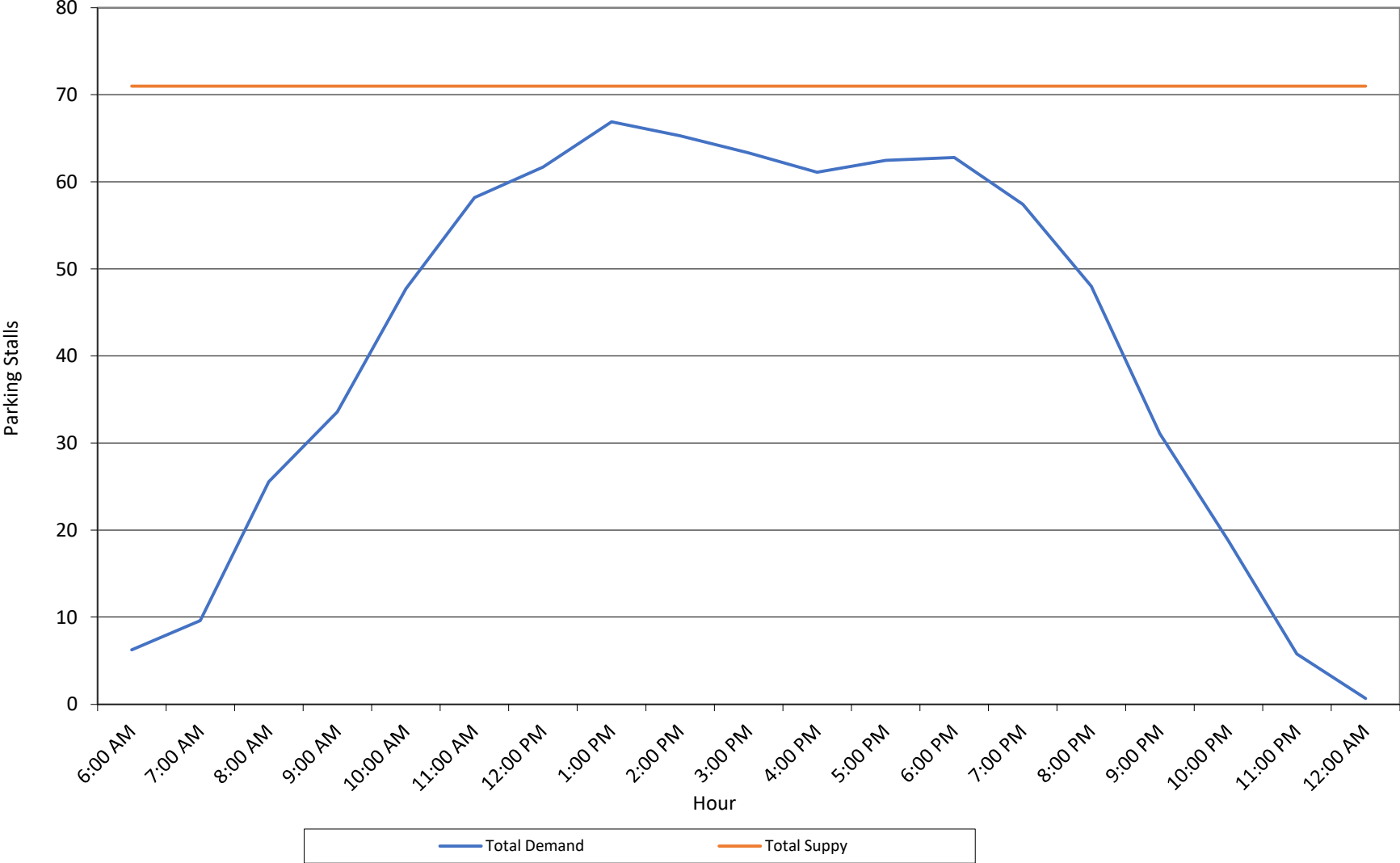
¹ The parking requirements are based on the City of Lomita Municipal Code Section 11-1.66.03 Parking Requirements.

Table 3
Peninsula Plaza
25820 Lucille Avenue Mixed-Use Building
Weekday & Weekend Peak Hour Parking Comparison

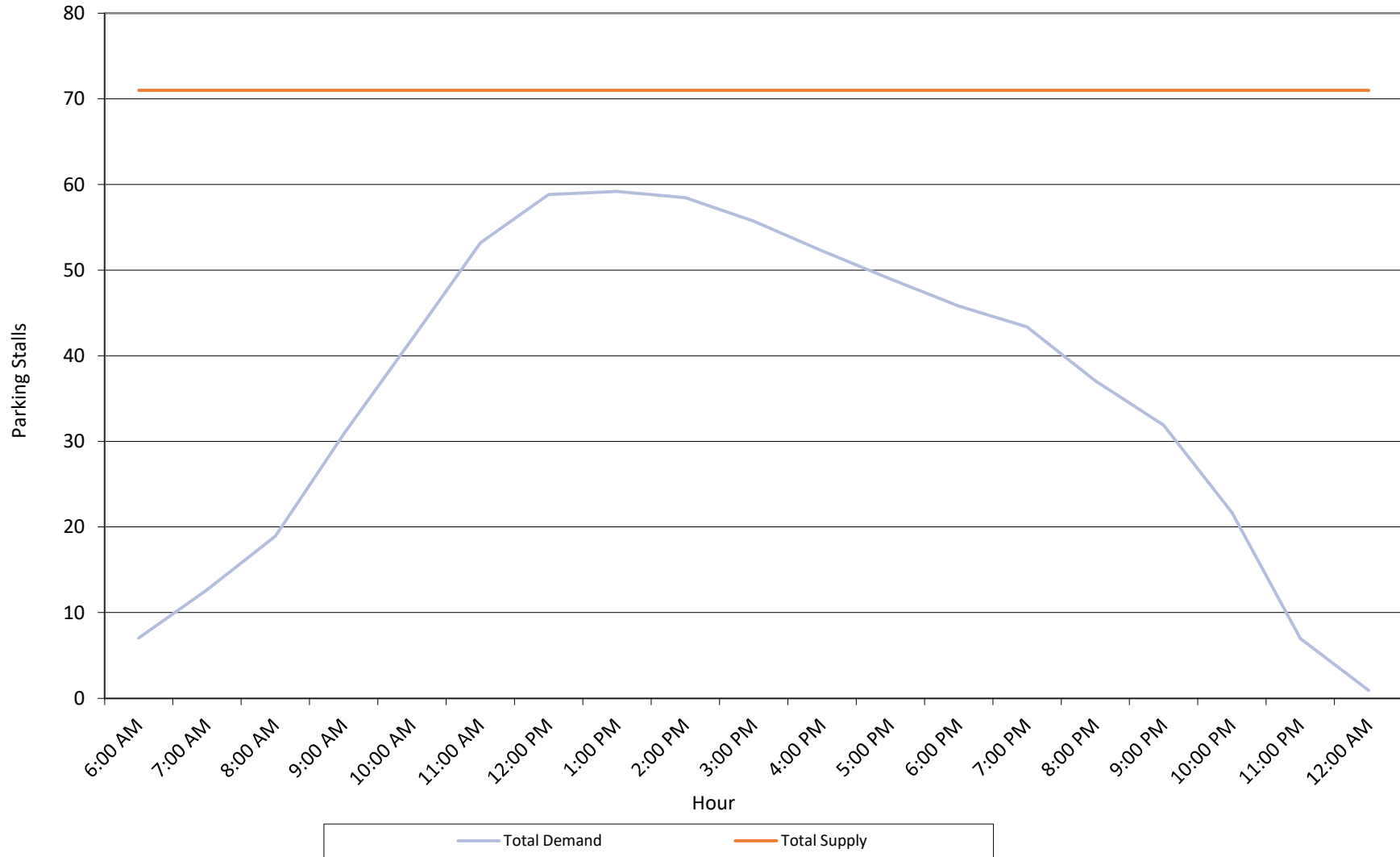
Monthly Comparison Summary								
Month	Weekday							
	Overall Pk		AM Peak Hr		PM Peak Hr		Eve Peak Hr	
	Time	Demand	Time	Demand	Time	Demand	Time	Demand
January	1 PM	53	11 AM	48	1 PM	53	6 PM	50
February	1 PM	54	11 AM	48	1 PM	54	6 PM	51
March	1 PM	58	11 AM	51	1 PM	58	6 PM	54
April	1 PM	57	11 AM	50	1 PM	57	6 PM	53
May	1 PM	58	11 AM	52	1 PM	58	6 PM	55
June	1 PM	59	11 AM	52	1 PM	59	6 PM	55
July	1 PM	57	11 AM	51	1 PM	57	6 PM	54
August	1 PM	58	11 AM	51	1 PM	58	6 PM	55
September	1 PM	56	11 AM	50	1 PM	56	6 PM	53
October	1 PM	57	11 AM	51	1 PM	57	6 PM	54
November	1 PM	59	11 AM	52	1 PM	59	6 PM	56
December	1 PM	67	11 AM	58	1 PM	67	6 PM	63
Late December	1 PM	60	11 AM	50	1 PM	60	6 PM	53

Monthly Comparison Summary								
Month	Weekend							
	Overall Pk		AM Peak Hr		PM Peak Hr		Eve Peak Hr	
	Time	Demand	Time	Demand	Time	Demand	Time	Demand
January	1 PM	46	11 AM	42	1 PM	46	6 PM	38
February	1 PM	46	11 AM	43	1 PM	46	6 PM	38
March	1 PM	50	11 AM	46	1 PM	50	6 PM	41
April	1 PM	49	11 AM	45	1 PM	49	6 PM	41
May	1 PM	51	11 AM	47	1 PM	51	6 PM	42
June	1 PM	51	11 AM	47	1 PM	51	6 PM	42
July	1 PM	50	11 AM	46	1 PM	50	6 PM	41
August	1 PM	51	11 AM	47	1 PM	51	6 PM	42
September	1 PM	49	11 AM	45	1 PM	49	6 PM	40
October	1 PM	50	11 AM	46	1 PM	50	6 PM	41
November	1 PM	52	11 AM	48	1 PM	52	6 PM	43
December	1 PM	59	11 AM	53	1 PM	59	6 PM	46
Late December	2 PM	54	11 AM	44	2 PM	54	6 PM	44

Peak Month Daily Parking Demand by Hour (Weekday)



Peak Month Daily Parking Demand by Hour (Weekend)



Appendices

Appendix A

ULI Shared Parking Manual, Third Edition



EXCEL MODEL INCLUDED

SHARED PARKING

THIRD EDITION



SHARED PARKING

THIRD EDITION

////////////////////////////////////

CUT COSTS FOR PARKING DEVELOPMENT

Avoid unnecessary costs and traffic by accurately estimating the parking requirements for mixed-use projects according to the types of tenants they will attract. Now in its third edition, this authoritative book has been updated throughout by author Mary S. Smith, a ULI member and leading parking expert, in collaboration with parking professionals and developers. It includes many additional land uses and revised parking ratios, and addresses trends such as increased use of ride sharing services and scooters. The Excel model version lets you plug in your project's land uses and calculates the number of parking spaces needed. This book is essential for developers, planners, government agencies, consultants, and engineers.



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Introduction

Shared parking is the use of a parking space to serve two or more individual land uses without conflict or encroachment. The ability to share parking spaces is the result of two conditions:

- Variations in the accumulation of vehicles by hour, by day, or by season at the individual land uses; and
- Relationships among the land uses that result in visiting multiple land uses on the same automobile trip.

Although this methodology for shared parking analysis was developed in the early 1980s,¹ the concept of shared parking was already well established: a fundamental principle of downtown planning from the earliest days of the automobile—indeed from the days of the horse and buggy—has always been to share parking resources rather than to allocate parking for each use or building. The resurgence of many central cities resulting from the addition of vibrant residential, retail, restaurant, and entertainment developments continues to rely heavily on shared parking for economic viability. Shopping centers have benefited from the addition of significant dining and entertainment venues, which draw pedestrian traffic (and parking) at other times of the year than the holiday shopping season, taking advantage of shared parking between retail and dining/entertainment uses. In addition, mixed-use projects in many different settings have benefited from shared parking.

Parking is a key element of any site development plan. Parking can consume 50 percent or

more of the building and land area of a development. An oversupply of parking can result in excess storm drainage impacts and unnecessarily high expenses (as of 2019, surface stalls can cost \$3,500 to \$6,000 per space, above-grade structured spaces \$15,000 to \$35,000, and underground parking spaces \$30,000 to \$50,000 or more). Insufficient parking can result in the intrusion of parking into neighborhoods or adjoining properties, excessive vehicle circulation, and unhappy users. Ultimately, great parking alone won't make a mixed-use project successful; however, inadequate or poorly designed parking can limit its potential success.

The most anti-sustainable approach of all is not to consider shared parking!

The triangular development in figure 1-1 has a “front row” of individual buildings including a pharmacy and two fast-food restaurants (toward the left in the photo); a middle row of strip retail (several of which are restaurants); and a third group of offices to the right that are primarily medical uses.

Reportedly, the local government required this design by forcing the developers to provide the two intermediate roads through the project, which extend for more than a mile through other commercial developments to the south.

A person who visits a doctor in the office buildings would drive and park at the pharmacy to pick up a prescription and then drive and park at the strip center to have lunch or pick up dry cleaning. If the exact same uses had been provided in a single two-story building, with the offices on the second floor over the retail at grade, the vehicle

FIGURE 1-1 A Wasted Opportunity for Shared Parking



would have been parked only once. With shared parking between these uses, the site could have accommodated at least 50 percent more of each use with adequate parking. This might have slowed the creeping commercial development of farmland along this corridor that occurred over the past 25 years.

An increasing concern today is the potential for future reductions in parking demand. Ride-hailing services offered by transportation network companies (TNCs) such as Uber and Lyft are already affecting parking requirements for many land uses, and significant potential exists for reduction in parking required if autonomous vehicles (AVs) become common.

The key goal of shared parking analysis, then, is to find the balance between providing adequate parking to support a development from

a commercial viewpoint while minimizing the negative aspects of excessive land area or resources devoted to parking and the potential for declining parking demand in the future. Mixed-use developments that share parking result in greater density, better pedestrian connections, and, in turn, reduced reliance on driving, typically because multiple destinations can be accessed by walking. Higher-density development, especially on infill sites in urban areas, is also more likely to support alternative modes of travel, including transit and carpools. Ironically, a critical element of pedestrian-oriented districts is adequate parking.

Concern for the negative impacts of growth stimulated a search for better ways to develop land. "Smart growth" is a collection of planning principles and strategies designed to facilitate economic development, strong communities, and

environmental health.² A particular focus of smart growth is to reduce sprawl. Smart growth projects typically are designed to create transportation options and reduce driving, especially for short trips. Walkable live/work/play environments, located near established mass transit connections and infrastructure in the city are central to the concept. Some communities are questioning the economic costs of abandoning infrastructure in the city only to rebuild it further out.³ A recent publication by the American Planning Association⁴ discusses the fiscal impacts for the community of urban density versus suburban low-density development and concludes as follows:

- Smart growth generally costs up to one-third less for upfront infrastructure.
- Smart growth saves an average of 10 percent in ongoing services such as police, ambulance, and fire service costs.
- Smart growth generates 10 times more tax revenue per acre.

A key principle of development today is known as "placemaking," which more strongly ties people to places. Placemaking typically focuses on the public spaces linking land uses to "capitalize on a local community's assets, inspiration, and potential, with the intention of creating public spaces that promote people's health, happiness, and well-being."⁵ ULI has recently undertaken an initiative known as the Creative Placemaking Project to "help build strong, healthy cities by promoting the integration of arts and culture in community revitalization."⁶

Another significant development type today is transit-oriented development (TOD), which seeks to cluster development near transit stations. With housing located within walking distance of rail transit, some trips and, in turn, some parking spaces can be eliminated.

Shared parking is a critical factor in the success of all these development approaches, and thus the importance of shared parking will continue to grow in future years. This report aims to provide parking consultants, planners, engineers, developers, and agencies with tools to better quantify and understand how shared parking can be successful.

OBJECTIVE OF THE THIRD EDITION

The widely accepted methodology for shared parking analysis was established in 1983 with the publication of the first edition of *Shared Parking*. In 2005, ULI and ICSC published a second edition, particularly to update and expand the land use categories and default factors. A decade later, the Parking Consultants Council (PCC) of the National Parking Association (NPA) proposed to ULI and ICSC that another update was due, given changes in society, transportation, and mixed-use development trends. The consensus was that the underlying concept and methodology are still viable but that an update of the default factors and addition of more land uses would be appropriate. Therefore, NPA's PCC was appointed by ULI and ICSC to update the publication, with NPA managing the process of producing the document. Subsequent to the start of the project, the Institute of Transportation Engineers (ITE) joined the effort, with the intent to coordinate updates to both *Shared Parking* and ITE's *Parking Generation Manual*⁷ (PGM, fifth edition, unless noted otherwise) database of observed parking ratios for individual land uses. Therefore, this third edition is truly a collaboration of the leading professionals in parking demand analysis as well as developers with a vested interest in "getting the number of parking spaces right." One might call this the "just enough, no regrets" parking supply for mixed-use developments.

Concededly, the number of parked cars per unit of land use on a design day falls within ranges, depending on the mix, types, and strength of the tenants of each land use. The recommendations herein are the *recommended parking supply* for these individual uses separately and when combined in a mixed-use project, particularly when planning and designing parking well before the development is fully leased.

As with any guidelines, the base ratios and other recommendations herein are a *starting point for analysis, subject to adjustment* to reflect professional judgment, market conditions, and regulatory requirements.

Appendix B

City of Lomita
Municipal Code Chapter 25.52

Sec. 11-1.66.03. Parking requirements.

For the purpose of this Article, unless otherwise stated, all requirements shall be calculated based on gross floor area.

<i>(A) Residential Parking Requirements:</i>	
One-family	Two (2) parking spaces in a garage, and one uncovered parking space for four (4) bedrooms and/or dens, and one additional uncovered parking space for five (5) or more bedrooms and/or dens.
Duplex, apartment house, condominiums,	Two (2) parking [spaces] in a garage for each unit of up to four (4) bedrooms and/or dens; three (3) spaces in a garage for five (5) or more bedrooms and/or dens, except as noted below. In addition, one guest parking space shall be required for each two (2) units. Said spaces shall not be located in the required front setback areas.
For the purpose of this section only, a bedroom or a den is defined as any room containing sixty (60) square feet or more that is not a living room, dining room, family room, kitchen or laundry area. Should any unit and/or units be enlarged in size by fifty (50) percent or more of its current size in any five-year period, the above requirements shall apply.	
Large family day care homes with no overnight care	Two (2) parking spaces in addition to the parking required for the residence, plus a safe drop off and pick up area either on or off site for a minimum of three (3) cars.
Large family and adult care homes for not more than seven (7) to twelve (12) children/adults providing twenty-four hours-per-day care	One (1) unenclosed parking space in addition to the required for the residence, plus one (1) space for each vehicle used directly in conducting of such use but not to exceed two (2) such vehicles. Tandem parking for the vehicles used in conducting the use is allowed.
<i>(B) Commercial and Other Uses—Shopping Centers:</i>	
Mixed use developments located on parcels of less than ten thousand (10,000) square feet of lot area	One (1) parking space for each one hundred seventy-five (175) square feet of gross floor area.
Mixed use developments located on parcels between ten thousand through fifty thousand (10,000—50,000) square feet of lot area	One (1) parking space for each two hundred (200) square feet of gross floor area.
Mixed use developments located on parcels of over fifty thousand (50,000) square feet of lot area	One (1) parking space for each two hundred twenty-five (225) square feet of gross floor area.
In a mixed use development, should a single use, other than offices, occupy fifty (50) percent or more of the development, the requirements for free standing uses shall apply to the entire development.	
<i>(C) Commercial and Other Uses—Freestanding:</i> The following minimum standards are set for specific types of developments with unique parking requirements where the uses are freestanding:	
<i>GENERAL COMMERCIAL:</i>	
Automobile rentals	To be determined by the planning commission, but in no case less than one space per two hundred fifty (250) square feet of gross floor area devoted to office plus one-third of the number of rental vehicles permitted by the commission.

Automobile, boat, trailer sales and rentals, plant nurseries and other open uses not in a building or structure	One (1) parking space for each one thousand (1,000) square feet of open area devoted to sales or display; provided, however, that where such area exceeds ten thousand (10,000) square feet, only one (1) space for each five thousand (5,000) square feet in excess of ten thousand (10,000) square feet shall be provided; plus one (1) space for two hundred fifty (250) square feet of gross floor area of buildings not devoted to display (i.e., office space).
Automobile service, body and repair facilities	Four (4) parking spaces for each service, body, and repair stall; and one (1) space for each two hundred fifty (250) square feet of building area not devoted to such stalls; a stall shall mean either an area specifically designed for work on one (1) automobile; or where there are open work areas which have not been so designed, the number of stalls shall be computed at the rate of one (1) parking space for each two hundred fifty (250) square feet of gross building area designated for service, body or repair work.
Banks, savings and loans, credit unions and financial institutions	One (1) parking space for each two hundred twenty-five (225) square feet of gross floor area, but not less than eight (8) parking spaces.
Business and professional uses and offices, including real estate, escrow, architect, income tax, and other professional uses but excluding professional training facilities	One (1) parking space for each three hundred (300) square feet of gross floor area, but not less than eight (8) parking spaces for new buildings.
Car wash, including gas pumps without servicing of cars	Six (6) 22-foot-long stacking spaces on site per wash bay, plus two (2) additional stacking spaces for each additional wash bay, plus three (3) parking spaces per each wash bay for drying of the cars and one (1) space for each two hundred fifty (250) square feet of gross building area devoted to office, cashier and other uses.
Contractors' offices, including plumbing, electrical, general, etc., where all business is conducted inside an office and where all materials are stored in an enclosed building	One (1) parking space for each two hundred fifty (250) square feet of gross building area, excluding storage areas, plus one (1) space for each four hundred (400) square feet of storage area and one (1) parking space for each vehicle operated or kept in connection with the business, not to exceed four (4) vehicles.
Furniture, large appliances, instruments, flower shops and similar uses, with display areas of five hundred (500) square feet or larger; where the display area is less than five hundred (500) square feet, parking requirements for retail commercial shall apply	One (1) parking space per each four hundred (400) square feet of gross floor area, but not less than eight (8) parking spaces
Hotels, motels	One (1) parking space for each sleeping unit, plus two (2) spaces for a resident manager and one (1) space for each two hundred fifty (250) square feet of office, lobby and other common areas. In addition, one (1) space for each one hundred (100) square feet of conference rooms, restaurants, bars or other places of

	assembly, not to exceed forty (40) spaces for the assembly area. Ten (10) percent of the above spaces shall be oversized measuring 10'x25'x14' vertical clearance.
Laundromats	One (1) parking space for each two (2) washing machines, based on the maximum number of washing machines in the establishment.
Retail takeout food establishments, where no food is consumed on premises	One (1) parking space for each three hundred (300) square feet of gross floor area, but not less than eight (8) spaces for new buildings.
Retail commercial including hardware, shoes, clothes, video stores and service commercial including beauty, nail, barber, and tanning salons; pet grooming and similar uses	One (1) parking space for each three hundred (300) square feet of gross floor area, but not less than eight (8) parking spaces for new buildings.
Sandwich shops, ice cream parlors, donut shops and similar uses as determined by the planning commission	One (1) parking space for each three hundred (300) square feet of gross floor area, but not less than eight (8) spaces for new buildings.
Fueling service stations with no other uses on the site	One (1) parking space for each two thousand (2,000) square feet of land area not devoted to any building or structure plus one (1) space per each two hundred fifty (250) square feet of gross floor area devoted to office, cashier and other uses.
Service stations with other uses on same site	Combined requirements for each individual use shall apply.
<i>Commercial recreation:</i>	
Arcades	One (1) parking space for each 150 square feet of gross floor area, plus bicycle racks, the number of which shall be determined by the planning commission.
Bowling alleys	Four (4) parking spaces per lane, plus one (1) space per one hundred (100) square feet of gross floor area used for food consumption or other recreation areas.
Ice and roller skating rinks	One (1) parking space for each four (4) persons based on the occupant load of the rink as determined by the building department, plus one (1) space for each one hundred (100) square feet of gross floor area used for food consumption or other recreation area.
Tennis, racquetball, handball courts	Two (2) parking spaces per court, plus one (1) space for each two hundred (250) square feet of gross floor area of offices, storage and other areas.
<i>Educational facilities:</i>	
Child or adult day care, pre-school, extended day care where no overnight care is provided	One (1) parking space per five (5) children/adults based on the maximum number of children/adults allowed on the operator's license, plus a safe drop off and pick-up area for a minimum of three (3) cars; plus one (1) space for each vehicle operated or kept in connection with the business, but not to exceed two (2) vehicles.

Elementary and junior high schools, private and public	Three (3) parking spaces per instructional classroom, plus one (1) space per two hundred fifty (250) square feet of gross floor area of the office areas.
Senior high schools	Six (6) parking spaces per instructional classroom, plus one (1) space per two hundred fifty (250) square feet of gross floor area of the office areas.
Colleges and universities	To be determined by the planning commission.
Trade schools, business colleges, commercial and professional schools	One (1) parking space for each three (3) persons based on the maximum occupant load as determined by the department of building and safety.
Where an auditorium is on the same site as the school, the required parking for an auditorium, plus fifty (50) percent of the above requirements shall be used to determine the required parking spaces for said school.	
<i>Health facilities:</i>	
Convalescent hospitals, nursing homes, group quarters, and similar uses	One (1) parking space for each two (2) residents based on the maximum number allowed on the operators' license plus two (2) spaces for a resident manager, plus one (1) space for each vehicle operated or kept in connection with the business, but not to exceed two (2) vehicles.
Hospitals	One and one-half (1½) parking spaces per patient bed.
Medical and dental offices, including outpatient clinics; veterinary clinics and hospitals	One (1) parking space for each two hundred twenty-five (225) square feet of gross floor area, but not less than eight (8) parking spaces.
<i>Places for public assembly:</i>	
Auditoriums, assembly halls, cultural centers, dance and fitness studios, health clubs and other similar uses	One (1) parking space for each three (3) persons based on the occupant load of the combined assembly areas, including stage and altar, as determined by the department of building and safety.
Above uses with schools, nursery or extended day care facilities	Same as above, plus two (2) parking spaces per classroom.
Bars, nightclubs and other similar establishments	One (1) parking space for each one hundred (100) square feet of gross floor area, but not less than ten (10) parking spaces.
Brewery with tasting room and brewpub as defined in Section 11-1.15.02(B)	One (1) parking space per four hundred (400) square feet of brewery, tasting area, kitchen, office and miscellaneous floor area, plus one (1) parking space for each one hundred fifty (150) square feet of dining area including outdoor dining area; and one (1) parking space for each three hundred (300) square feet of retail floor area.
	The planning commission may consider spaces in a municipal parking lot which is within five hundred (500) feet of the subject property for part of the parking requirement.
Restaurants and other places where food and beverages are served for on-site consumption	One (1) parking space for each one hundred fifty (150) square feet of gross floor area including outdoor dining areas, but not less than ten (10) parking spaces.
	The planning commission may consider spaces in a municipal parking lot which is within five hundred

	(500) feet of the subject property for part of the parking requirement.
<i>Manufacturing and other uses:</i>	
Manufacturing and warehousing	One (1) parking space per four hundred (400) square feet of gross building area and one (1) space for each vehicle operated or kept in connection with the use.
Public utility facilities	To be determined by the planning commission.
Self-storage facilities	One (1) space for each one thousand five hundred (1,500) square feet of gross floor area. The planning commission may consider a reduction to this requirement base on a parking study.
Uses otherwise not provided for	To be determined by the planning commission.

(Ord. No. 475, § 1, 2-4-91; Ord. No. 498, § 2, 7-20-92; Ord. No. 529, §§ 1—4, 11-21-94; Ord. No. 568, § 1, 10-21-96; Ord. No. 606, § 2, 5-3-99; Ord. No. 665, pt. 1, 6-21-04; Ord. No. 748, § 2(pt. 19), 1-17-12; Ord. No. 757, § 2, 6-3-13; Ord. No. 787, § 6, 5-2-17; Ord. No. 798, § 4, 5-15-18; Ord. No. 819, § 2, 12-1-20; Ord. No. 828, § 4, 1-18-22)

Sec. 11-1.66.07. Required dimensions and parking lot design.

(A) *Design of parking spaces:*

- (1) Parking spaces arranged at ninety (90) degrees to parking aisles shall be not less than set forth:

Type of Use

Dimensions

	Width in Feet	Length in Feet
Residential	9	20
Nonresidential	9	19

- (2) Parking spaces adjacent to buildings, fences, walls, trash enclosures and similar structures shall be ten (10) feet wide.
- (3) Parking spaces in a residential garage shall be ten (10) feet wide, twenty (20) feet long, and have a vertical clearance of a minimum of seven (7) feet except when otherwise required by the building code, or as modified by the planning director and/or planning commission.
- (4) Parking spaces for other than ninety-degree angles shall be constructed in accordance with the attached figures.

(B) *Parking Aisle:*

- (1) Parking aisles for ninety-degree spaces shall be a minimum of twenty-five (25) feet wide. Parking aisles for other than ninety-degree parking shall be in accordance with the attached figures [following this section].
- (2) A ten-foot-wide turnaround area shall be provided at the end of a single access parking aisle. Said area shall be clear, unobstructed and free of any storage, debris and other materials or vehicles and shall be marked "NO PARKING" (figure P3).

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- (3) The clearance between a detached rear garage and the house shall be a minimum of twenty-five (25) feet when the house overlaps the parking entrance of the garage by ten (10) feet or more. The clearance can be reduced by one foot for each foot overlap less than ten (10) feet (figure P4).
- (C) *Driveways and Driveway Depressions:* The following dimensions for driveways and driveway depressions are required by the Los Angeles County Fire Department. Unless modified by the fire department, city planning director and/or planning commission, the dimensions shall be as follows:
- (1) *Residential:*
- (a) For two (2) or less units constructed on a lot or parcel of ground, no portion of which is more than one hundred fifty (150) feet from a dedicated street or private and future street, a driveway and driveway depression not less than twelve (12) feet wide, but not to exceed sixteen (16) feet in width for a two-car garage, is required. For an attached three-car garage or larger, located at the front of the building, a twenty-foot-wide driveway and driveway depression may be provided.
 - (b) For two (2) or less units constructed on a lot or parcel of land, any portion of which is more than one hundred fifty (150) feet from a dedicated street or private and future street, a driveway and driveway depression twenty (20) feet in width shall be provided.
 - (c) For three (3) or more units constructed on a lot or parcel of land, any portion of which is less than one hundred (150) feet from a dedicated street or private and future street, a driveway and driveway depression twenty (20) feet in width shall be provided.
 - (d) For three (3) or more units constructed on a lot or parcel of land, any portion of which is more than one hundred fifty (150) feet from a dedicated street or private and future street, a driveway and driveway depression twenty-six (26) feet in width shall be provided.
 - (e) The required driveways shall be unobstructed in width and clear to the sky.
- (2) *Nonresidential:*
- (a) The minimum required driveway and driveway depression for one-way vehicular traffic shall be fifteen (15) feet in width where structures are located within one hundred fifty (150) feet from a dedicated street or private and future street and a minimum of twenty (20) feet where structures are located one hundred fifty (150) or more feet from said street.
 - (b) The minimum required driveway and driveway depression for two-way vehicular traffic shall be twenty (20) feet wide where structures are located within one hundred fifty (150) feet from a dedicated street or private and future street and minimum of twenty-six (26) feet where structures are located one hundred fifty (150) or more feet from said street, except as specified below.
 - (c) A thirty-foot-wide driveway and driveway depression for two-way vehicular traffic shall be required on major thoroughfares, including Pacific Coast Highway, Lomita Boulevard, Narbonne Avenue, Western Avenue and Palos Verdes Drive North.
 - (d) The required driveways shall be unobstructed in width and clear to the sky.
 - (e) Driveway approaches, for both commercial and residential developments, shall be constructed pursuant to the specifications of the Los Angeles County Public Works Department. Further, whenever feasible, the curb return adjacent to the driveway depression shall exceed the standard requirements.

(Ord. No. 475, § 1, 2-4-91)

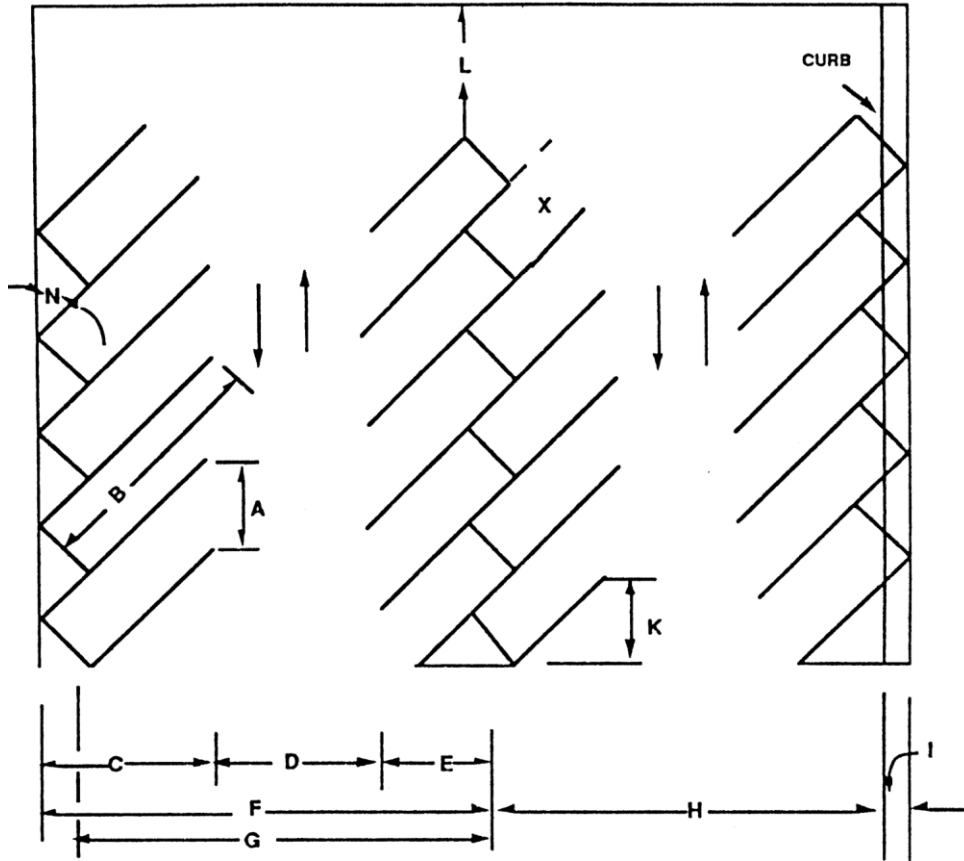
Parking Illustration

× = Stall Not Accessible in Certain Layouts

Parking layout dimensions (in ft) for 9 ft × 19 ft stalls at various angles

Dimension	On Diagram	Angle N				
		0°	30°	45°	60°	90°
Stall width, parallel to aisle	A	—	18	12.7	10.4	9
Stall length of line	B	22	34.6	28	24.2	19
Stall depth to wall	C	9	17.3	19.5	20.5	19
Aisle width between stall lines	D	12	12	12	16	25
Stall depth, interlock	E	9	13.4	16.6	18.5	19
Module, wall to interlock	F	30	42.7	51	55	63
Module, interlocking	G	30	38.8	47.8	53	63
Module, interlock to curb face	H	30	41.4	48.2	63.2	61
Bumper overhang (typical)	I	—	1.3	2.0	2.3	2.5
Setback	K	—	16.4	13.1	9.5	—
Cross aisle, one-way	L	14	14	14	14	14
Cross aisle, two-way	—	25	25	25	25	25

***Parking
Principles,***



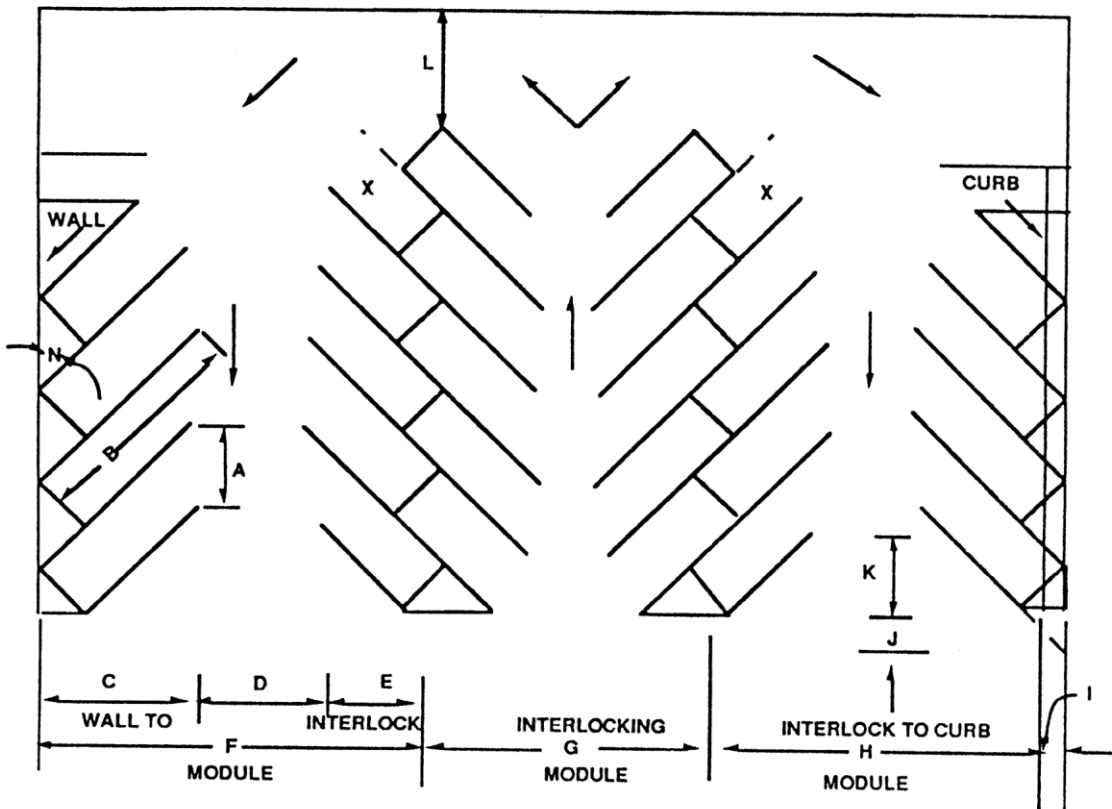
Parking Illustration

Parking layout dimensions (in ft) for 9 ft x 19 ft stalls at various angles

Dimension	On Diagram	Angle N				
		0°	30°	45°	60°	90°
Stall width, parallel to aisle	A	—	18	12.7	10.4	9
Stall length of line	B	22	34.6	28	24.2	19
Stall depth to wall	C	9	17.3	19.5	20.5	19
Aisle width between stall lines	D	25	25	25	25	25
Stall depth, interlock	E	9	13.4	16.6	18.5	19
Module, wall to interlock	F	43	55.7	61.8	64.5	63
Module, interlocking	G	43	51.8	58.2	62.0	63

Module, interlock to curb face	H	43	54.4	59.1	61.7	60.1
Bumper overhang (typical)	I	—	1.3	2.0	2.3	2.1
Setback	K	—	16.4	13.1	9.3	—
Cross aisle, one-way	L	14	14	14	14	14
Cross aisle, two-way	—	25	25	25	25	25

Parking Principles,



Parking Illustration

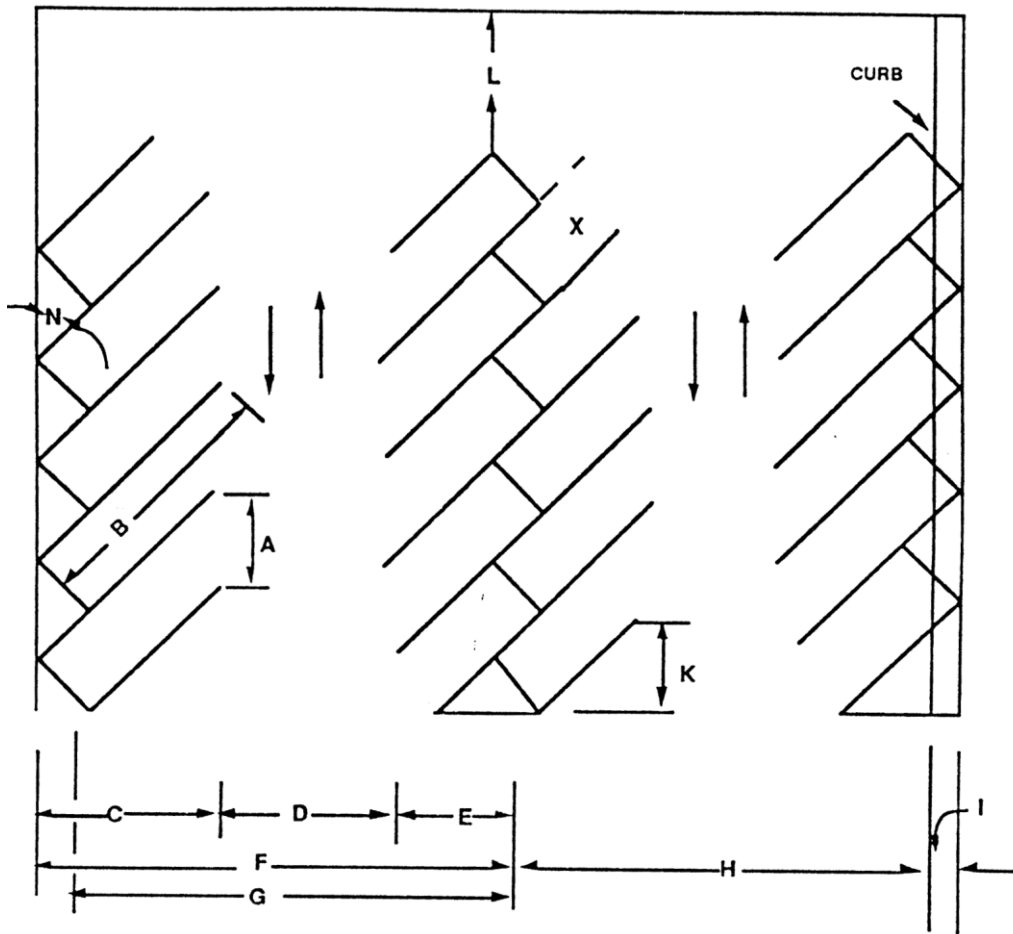
x = Stall Not Accessible in Certain Layouts

Parking layout dimensions (in ft) for 9 ft x 20 ft stalls at various angles

	On	Angle N
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Dimension	Diagram	0°	30°	45°	60°	90°
Stall width, parallel to aisle	A	—	18	12.7	10.4	9
Stall length of line	B	22	35.6	29	25.2	20
Stall depth to wall	C	9	17.8	20.5	21.8	20
Aisle width between stall lines	D	12	12	12	16	25
Stall depth, interlock	E	9	13.9	17.3	19.6	20
Module, wall to interlock	F	30	43.7	49.8	57.4	65
Module, interlocking	G	30	39.8	46.6	55.2	65
Module, interlock to curb face	H	30	42.6	47.6	55.1	63
Bumper overhang (typical)	I	—	1.3	2.0	2.3	2.5
Setback	K	—	17.3	14.1	10	—
Cross aisle, one-way	L	14	14	14	14	14
Cross aisle, two-way	—	25	25	25	25	25

***Parking
Principles,***



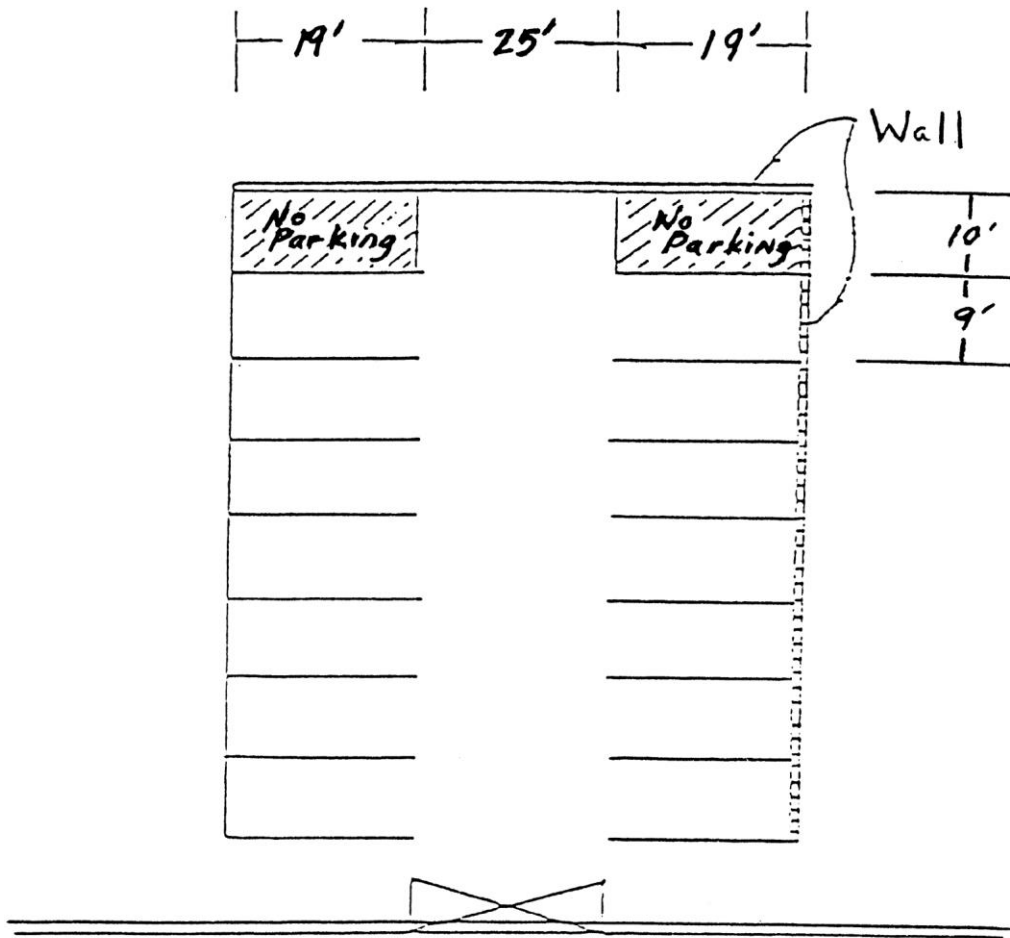
Parking Illustration

Parking layout dimensions (in ft) for 9 ft x 20 ft stalls at various angles

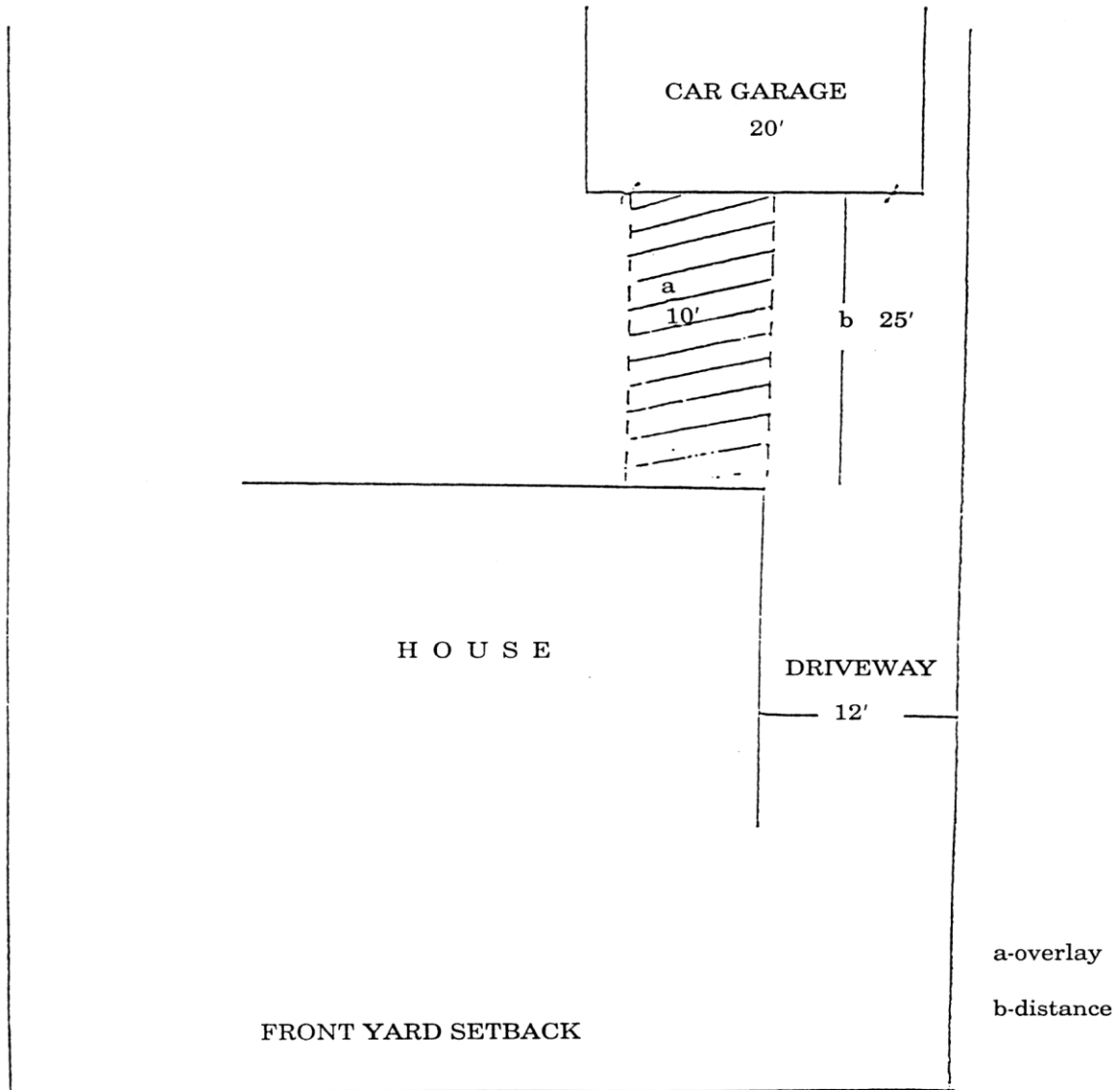
Dimension	On Diagram	Angle N				
		0°	30°	45°	60°	90°
Stall width, parallel to aisle	A	—	18	12.7	10.4	9
Stall length of line	B	22	35.6	29	25.2	20
Stall depth to wall	C	9	17.8	20.5	21.8	20
Aisle width between stall lines	D	25	25	25	25	25
Stall depth, interlock	E	9	13.9	17.3	19.6	20
Module, wall to interlock	F	43	56.7	62.8	66.4	65

Module, interlocking	G	43	52.8	59.6	64.2	65
Module, interlock to curb face	H	43	55.4	60.8	64.1	62.5
Bumper overhang (typical)	I	—	1.3	2.0	2.3	2.5
Setback	K	—	17.3	14.1	10	—
Cross aisle, one-way	L	14	14	14	14	14
	On	Angle N				
Dimension	Diagram	0°	30°	45°	60°	90°
Cross aisle, two-way	—	25	25	25	25	25

***Parking
Principles,***



Parking Illustration



Parking Illustration

Figure P4. Distance Between House and Detached Rear Garage

Sec. 11-1.66.08. Standards for developing parking facilities.

The following development standards shall be the minimum required for parking facilities:

- (A) Except for single-family parking facility, parking spaces shall be arranged to permit vehicular traffic to move into and out of a parking area without backing onto a street, sidewalk or highway, except that an alley may be used for turning into and out of a parking space.
- (B) The entrances and exits to parking areas shall be clearly marked. One-way driveways shall have directional signs placed on the pavement.

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- (C) All parking spaces shall be double striped to facilitate the movement in and out of the parking stall.
 - (D) Parking areas and spaces in a commercial garage or "tucked" under a building shall have a vertical clearance of not less than eight (8) feet, two (2) inches.
 - (E) Columns, pillars and other obstructions in a parking facility shall not encroach into the required dimensions of any parking space, aisle or driveway.
 - (F) Handicapped parking spaces shall be provided, marked and posted as required by the state.
 - (G) Wheelstops shall be provided for all parking spaces and located to prevent encroachment of cars over walkways, sidewalks, landscaped areas, etc. Wheelstops shall not be required in residential garages.
 - (H) Commercial subterranean garages, fully or partially underground, and aboveground commercial parking structures shall be subject to planning commission review and approval. Said garages shall be subject to design standards, setbacks and landscaping requirements as determined by the planning commission, and shall at minimum meet the requirements of this article. The height of the garages above the average natural grade and/or garage and structure above it shall not exceed the maximum allowed height of the zone in which such garage is located.
 - (I) Where a commercial development or parking facility is adjacent to a residential zone or use, a solid masonry wall not less than six (6) feet in height shall be constructed along the lot line adjacent to said zone or use, except that said wall shall step down to a maximum of forty-two (42) inches in height for a distance equal to the required front setback of an adjoining residential use.
 - (J) Where a wall of a commercial facility is constructed along a commercial driveway, said wall shall step down to a maximum of forty-two (42) inches in height for a distance of ten (10) feet from the property line.
 - (K) Where a commercial or a residential driveway is proposed along an existing wall/fence which is more than forty-two (42) inches high, the design of such driveway shall be satisfactory to the city. Further, it shall be located so that visibility of the vehicular and pedestrian traffic is not compromised.
 - (L) Where fences or walls are constructed for commercial parking areas along the front property line of an interior lot, or front and side property line on a corner lot, such wall or fence shall not exceed forty-two (42) inches in height. The height of such wall or fence for auto-related uses, contractor's yards and similar uses shall be determined by the planning commission.
 - (M) Where plants are used along property lines of commercial developments for screening, such plants shall be maintained at a height not to exceed the height of walls or fences permitted in the same location, unless otherwise approved by the planning commission.
 - (N) Lighting of parking and driveway areas shall be required. It shall be arranged so as to reflect the light away from any adjoining property. The lights shall be of energy efficient type, and be vandal resistant.
 - (O) All parking facilities, including parking spaces, aisles, driveways, etc., shall be constructed pursuant to Los Angeles County specifications.

(Ord. No. 475, § 1, 2-4-91; Ord. No. 763, § 2, 1-6-14)

Sec. 11-1.66.09. Location of parking, storage and loading facilities.

- (a) *Ownership:* Property on which required parking is established shall be under the same ownership as the use it is intended to serve. Any joint ownership resulting from the establishment of collective parking facilities, as provided herein, shall be construed as complying with this provision.
- (b) *Residential uses:*

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- (1) Required parking facilities for residential uses as specified in subsection (A) of section 11-1.66.03 (Parking Requirements) shall be located on the same lot or parcel of land as the use the parking facilities are intended to serve. Such facilities shall be conveniently accessible.
 - (2) Storage and/or parking of detached camper tops/shells, dismantled and inoperative vehicles and other auto and recreation equipment items shall not be permitted in the front-yard area. In addition, storage in the front-yard area of any materials (construction, lumber, metals, plastic, etc.), fixtures, appliances, machines, trash or waste, or other materials which are not customarily considered as decorative landscaping features are hereby prohibited.
 - (3) Storage and/or parking of boats, trailers, other vehicles and similar equipment as well as camper tops, auto- and recreation-related and domestic items in the rear and side yard areas shall be screened by a solid wall or opaque fence six (6) feet high to minimize any undesirable appearance from the street and surrounding properties. On the street side of corner lots, no storage and/or parking shall be permitted closer to the street than the required front yard setback of the adjacent interior lot.
 - (4) Parking pads or spaces other than a driveway shall not be permitted in the front-yard area unless they have been reviewed and approved by the planning commission pursuant to Article 70, "Zoning Ordinance Administration", and are paved pursuant to Los Angeles County specifications for residential driveways. Only recreation equipment and/or operable vehicles may be stored/parked on such approved pads.
 - (5) Storage of any commercial or construction equipment, materials, or nursery stock or storage and/or parking of boats, house trailers, camper trailers, detached camper tops, vehicles or dismantled vehicles and similar items on vacant lots shall not be permitted, except for materials and equipment being used for construction on the premises where a valid building permit has been issued or applied for.
 - (6) House trailers, motor homes, mobile homes, campers, boats and similar uses shall not be occupied or used as a dwelling unit in any land use district unless located in a trailer or mobile home park which has been approved by the City of Lomita.
 - (7) Operative automobiles and motor vehicles may be parked or stored in the front yard setback area only on an approved driveway which leads directly to a garage.
 - (8) Dismantled and/or inoperative vehicles may not be parked or stored in a location which prevents access to an approved driveway leading to a garage and shall be screened by a solid wall or opaque fence six (6) feet high to minimize any undesirable aesthetic impacts from the street and surrounding properties. On the street side of corner lots, storage and/or parking of dismantled and inoperative vehicles shall not be permitted closer to the street than the required front yard setback of the adjacent interior lot.
- (c) *Uses other than residential:*
- (1) *Parking facilities:* Required parking facilities for uses other than residential shall be located:
 - (a) On the same lot or parcel of land as the use such parking facilities are intended to serve; or
 - (b) On a lot or parcel of land held under joint ownership, provided such parking facilities are located adjoining the use or uses served and contiguous for a distance not less than twenty (20) feet; [or]
 - (c) On a parcel of land separated only by an alley from the lot or parcel of land which the use or uses served are located provided:
 1. That said lots or parcels of land are in the same or joint ownership and separated only by an alley; and
 2. That said lots or parcels of land would be contiguous if not separated by said alley, for a distance of not less than twenty (20) feet; and

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3. That the direct vehicular passage between said lots or parcels of land would be possible in conformance with section 11-1.66.07; and
 4. That such parking facilities are in close proximity to the actual use or uses served.
- (2) Storage of any commercial or construction equipment, materials, or nursery stock or storage and/or parking of boats, house trailers, camper trailers, detached camper tops, vehicles or dismantled vehicles on vacant lots shall not be permitted, except for materials and equipment being used for construction on the premises where a valid building permit has been issued or applied for, and where such use has been approved pursuant to the Lomita Zoning Ordinance.
 - (3) Storage and/or parking of bins, steel or other storage containers, except for loading vehicles as permitted in section 11-1.66.04(D) and recycling containers, is hereby prohibited.
 - (4) *Reduction or encroachment:*
 - (a) Land within the right-of-way of a proposed street or highway, or within the planned ultimate right-of-way of a street or highway proposed to be widened, shall not be used to provide required parking or loading facilities.
 - (b) Required parking or loading facilities may not be reduced or encroached upon except upon approval by the planning commission and subject to the provisions of Article 70, "Variances and Conditional Use Permits."
 - (5) *Combined parking or loading facilities:*
 - (a) Required parking facilities may be provided collectively for two (2) or more buildings or uses located on separate lots or parcels of land provided that the total area of such facilities does not equal less than the combined requirements for the individual uses.
 - (b) Parking and loading facilities designated for one use may not be counted or considered as also providing required parking or loading facilities for any other use.

(Ord. No. 498, § 2, 7-20-92; Ord. No. 701, § 2(pt. 1), 6-4-07; Ord. No. 733, § 2(pt. 2), 9-7-10)

Sec. 11-1.66.10. Approval procedures.

A site plan shall be submitted to the planning commission pursuant to the provisions of Article 70, "Zoning Ordinance Administration," prior to the establishment of any required parking facilities for three (3) or more motor vehicles, except parking for single-family residences. Said plan shall contain a detailed parking arrangement, accurately dimensioned, showing individual parking spaces, aisles and driveways, adequate ingress and egress, and all other requirements of this Article.

(Ord. No. 475, § 1, 2-4-91; Ord. No. 733, § 2(pt. 2), 9-7-10)